

SmartCode

v.10.0

Municipality

Element 1 General Provisions	01	Element 5 Zoning Regulations	25
1.1 Authority	01	5.1 Buildings and Yards	25
1.2 Applicability	01	5.2 Frontage Requirements	26
1.3 Intent	02	5.3 Use	33
1.4 Transect Districts Established.	04	5.4 Specific Use Restrictions	35
1.5 Nonconformities	05	5.5 Parking Standards	36
1.6 Enforcement	06	5.6 Landscaping Standards	41
1.7 Severability	06	5.7 Fencing	42
1.8 Effective date	06	5.8 Signage Standards	42
1.9 Repealer	06	5.9 Special Conditions	44
Element 2 Comprehensive Plan	07	Element 6 Right-of-way Regulations	51
2.1 Sector Plan	07	6.1 General	51
2.2 Sectors	07	6.2 Context	51
2.3 Community Unit Types	07	6.3 Right-of-way Assemblies.	52
Element 3 Land Subdivision	11	6.4 Roadway Requirements	53
3.1 Community Unit Standards.	11	6.5 Public Frontage Requirements	57
3.2 Transportation Standards	11	6.6 Alleys.	61
3.3 Block Standards	12	Element 7 Administration and Process	67
3.4 Zoning Standards	12	7.1 Approval Authority	67
3.5 Platting Standards.	13	7.2 Amendments.	68
3.6 Civic Space Standards	13	7.3 Notification and hearings	69
3.7 Special Requirements.	14	7.4 Processes.	69
Element 4 Special Area Regulations	19	7.5 Applications.	74
4.1 General	19	7.6 Deviations.	78
4.2 Special Districts	19	Element 8 Definitions.	81
4.3 Transit Overlay Districts (TOD)	19	8.1 Definitions	81
4.4 Pink Zone Overlay District (PZ)	22		

Municipality

ELEMENT 1 GENERAL PROVISIONS**1.1 Authority**

- 1.1.1 The action of the **municipality, state** in the adoption of this ordinance is authorized under the **charter of the municipality, § sec. and state statutes, § sec.** et seq.
- 1.1.2 Enactment. For the purposes cited above and for the general purposes of promoting the health, safety, and general welfare of the citizens of the **municipality**, the **municipal legislature** ordains, adopts and enacts this chapter in its entirety, including text, map, and all regulations, all of which will be known as the **municipal SmartCode**. This **chapter** applies to land within the **municipal** limits of the **municipality**, as shown on the official zoning map or described by ordinance.
- 1.1.3 This **chapter** is adopted as one of the instruments of implementation of the public purposes and objectives of the Comprehensive Plan. This **chapter** is declared to be in accord with the Comprehensive Plan.

1.2 Applicability

- 1.2.1 Conflicting ordinances. All ordinances or parts of ordinances in conflict with this **chapter** or inconsistent with its provisions, including, but not limited to, **chapters (zoning, subdivision, signage, landscaping)** of the **municipal code of ordinances**, are hereby repealed and superseded to the full extent necessary to give this **chapter** full force and effect.
- 1.2.2 Rules of construction. The following general rules apply to regulations governing this chapter:
 - a. Numerical metrics take precedence over graphic metrics;
 - b. The diagrams and illustrations within this **chapter**, unless specifically noted as advisory, are considered regulatory in nature and are legally binding; and
 - c. The definition of a term in this **chapter** takes precedence over the definition of the same term elsewhere in the **municipal code of ordinances**.
 - i. Terms used throughout this **chapter** may be defined in **Element 8 Definitions**. Those terms not defined in **Element 8 Definitions** are accorded their commonly accepted meanings.

Calibration

Throughout the Elements, text will be identified that must be calibrated to the local context. This will be flagged with **green** text. Most of the numeric regulations should be locally calibrated to reflect the character and context. These will not be identified as it is assumed they will all be considered and adjusted as necessary.

Modifiers

Throughout the Elements various methods will be presented that may modify the base SmartCode Framework. These will be coded with **blue** text and discussed as necessary within the sidebar.

1.3 Intent

1.3.1 The intent and purpose of this [chapter](#) is to enable, encourage and qualify the implementation of the following policies:

1.3.2 [The climate](#)

- a. [That the region and its' communities should be arranged to reduce automobile use.](#)
- b. [That areas of current and future hazard should remain or become undeveloped to avoid unnecessary loss of life and community wealth.](#)
- c. [That prime agricultural land should be preserved to protect future food supplies.](#)
- d. [That regional watersheds should be protected to preserve the future water supply.](#)
- e. [That communities, blocks, and buildings should reinforce social fabric to support local decision-making and collective response to natural disasters.](#)
- f. [That regions and communities in areas of least climate risk should prepare to receive people migrating away from areas of climate hazard, and accommodate this growth in a climate-responsive manner.](#)

1.3.3 The region

- a. That the region should retain its natural infrastructure and visual character.
- b. That growth strategies should encourage infill and redevelopment in parity with new communities.
- c. That development within or contiguous to urban areas should be structured in the pattern of Urban Center (UCT), Town Center (TCT), or Village (VIL), and be integrated with the existing urban pattern.
- d. That development non-contiguous to urban areas should be organized in the pattern of Hamlet (HAM) or Village (VIL).
- e. [That affordable housing should be distributed throughout the region to match job opportunities and to avoid concentrations of poverty.](#)
- f. That transportation corridors should be planned and reserved in coordination with land use.
- g. That green corridors should be used to define and connect the urbanized areas.
- h. That the region should include a framework of transit, pedestrian, and bicycle systems that provide alternatives to the automobile.

Climate Intent

As local governments develop climate action plans, they will identify critical issues concerning their own resiliency. This intent is general in nature and will need to be crafted for the local and regional context.

Affordability

Each local government will need to adjust the affordable housing intent to reflect their own [comprehensive plan](#) policies.

Municipality

1.3.4 The community

- a. That neighborhoods, corridors, town centers, and urban centers should be compact, pedestrian-oriented and mixed use.
- b. That neighborhoods, town centers, and urban centers should be the preferred pattern of development and that districts specializing in a single use should be the exception.
- c. That ordinary activities of daily living should occur within walking distance of most dwellings, as much as is feasible, allowing independence to those who do not drive.
- d. That interconnected networks of thoroughfares should be designed to disperse traffic and reduce the length of automobile trips.
- e. That within neighborhoods, a range of housing types and price levels should be provided to accommodate diverse ages and incomes.
- f. That appropriate building densities and land uses should be provided within walking distance of transit stops.
- g. That civic, institutional, and commercial activity should be embedded in the town center and neighborhoods, not isolated in remote single-use complexes.
- h. That schools should be sized and located to enable children to walk or bicycle to them.
- i. That a range of open space should be distributed within neighborhoods and the town center.

Transit

This intent will need to be deleted or expanded depending upon transit availability.

1.3.5 The block and the building

- a. That buildings and landscaping should contribute to the physical definition of thoroughfares as civic places.
- b. That development should adequately accommodate automobiles while prioritizing the pedestrian, the bicyclist, and the spatial form of public areas.
- c. That architecture and landscape design should grow from local climate, topography, history, and building practice.
- d. That buildings should provide their inhabitants with a clear sense of geography and climate through energy efficient methods.
- e. That civic buildings and public gathering places should be provided as locations that reinforce community identity.

- f. That civic buildings should be distinctive and appropriate to a role more important than the other buildings that constitute the fabric of the city.
- g. That the preservation and renewal of historic buildings should be facilitated, to affirm the continuity and evolution of society.

1.3.6 The transect

- a. That communities should provide meaningful choices in living arrangements as manifested by distinct physical environments.
- b. That the transect district descriptions in 1.4 Transect Districts Established constitute the intent of this chapter with regard to the general character of each of these environments.

1.4 Transect Districts Established

1.4.1 Zoning under this chapter is limited to the following transect district designations.

- a. **T1 Natural (T1):** This district consists of lands approximating or reverting to a wilderness condition, including lands unsuitable for settlement due to topography, hydrology or vegetation. Public parks may occur, but general development is prohibited.
- b. **T2 Rural (T2):** This district consists of sparsely settled lands in open or cultivated states. These include woodland, agricultural land, rangeland, and pasture. Typical buildings are farmhouses and agricultural buildings. Roads are rural in character and have no pedestrian facilities. Landscaping is naturally occurring.
- c. **T3 Sub-Urban (T3):** This district consists of low density residential areas, with some mix of use, home occupations and accessory buildings. Street and yard planting are naturalistic and building setbacks are relatively deep. Blocks may be large and thoroughfare networks irregular.
- d. **T4 General Urban (T4):** This district includes a mix of uses but is primarily mixed density residential. It may have a wide range of building types: houses, townhouses, duplexes, small apartment buildings, and live-work units. Setbacks and landscaping are variable. Streets and sidewalks define medium-sized blocks.
- e. **T5 Urban Center (T5):** This district consists of higher density mixed-use buildings that accommodate retail, offices, townhouses, and apartments. It has a tight network of thoroughfares, with wide sidewalks, regularly spaced street tree planting and buildings set close to the sidewalks.

Districts

Most codes will not include all 6 transect districts along with special districts. For example, if the code is just for an urban neighborhood, it likely will not include T1, T2, T6, or SD. If the code is for a small town, it likely will not include T6. This entire section must be locally calibrated.

Terminology

Throughout the SmartCode the term transect district may be replaced with transect sub-district, transect zone, transect sub-zone, or character zone depending upon the way this code is applied. Where the SmartCode is applied as a stand-alone zoning and land development code, the terms transect zone or transect district are appropriate. Where the SmartCode is applied as an optional or parallel code, the terms transect sub-zone or transect sub-district are appropriate. Using the term zone versus the term district may have additional legal consequences. Consult with a zoning attorney familiar with state and local laws for the correct terminology. For the purpose of the SmartCode Framework, the term district will be used exclusively.

Municipality

- f. **T6 Urban Core (T6):** This district consists of the highest density and height, with the greatest variety of uses, and civic buildings of regional importance. It may have larger blocks; streets have steady street tree planting and buildings set close to the wide sidewalks. Typically only large towns and cities have an Urban Core.
 - g. **Special District (SD):** Special districts are designated for uses and configurations that are incompatible with Transect districts, but that are consistent with Section 1.2 Intent.
- 1.4.2 **All or a portion of the property within a designated Transect zone may be further subject to the following overlay districts:**
 - a. **Transit Overlay District (TOD):** an area within a short walking distance of a commuter rail, light rail, or bus rapid-transit station. The TOD modifies transect zones by increasing density and height, and reducing parking requirements;
 - b. **Historic Preservation District:** an area designated as historically significant, whether designated locally, at the state, or federal levels. The Historic Preservation District modifies transect zones by reflecting the heights and setbacks of the existing historic patterns; and
 - c. **Pink Zones:** an area designated for enabling small-scale, community-centered development and revitalization. It provides a platform to enhance community character and allow existing businesses and residents to remain and profit from the improved quality of life. It lightens regulation and speeds building in areas targeted for redevelopment or revitalization.

1.5 Nonconformities

- 1.5.1 Any nonconforming structure with a certificate of occupancy may continue to be occupied, operated, repaired and renovated.
 - a. The modification of existing buildings is permitted by right if such changes result in greater conformance with the specifications of this **chapter**.
- 1.5.2 Additions to an existing nonconforming structure must conform to the requirements of this ordinance.
- 1.5.3 The restoration or rehabilitation of an existing building will not require the provision of:
 - a. Parking in addition to that existing; and
 - b. **On-site stormwater retention/detention in addition to that existing, unless otherwise required by county or state code.**

Overlays

Nuance may be added to transect districts with overlays if needed. These are most commonly used for TOD areas and historic districts.

1.6 Enforcement

- 1.6.1 Should a violation of an approved application occur during construction, or should any construction, site work, or development be commenced without an approved application, the municipality has the right to require the owner to stop, remove, and mitigate the violation, or to secure a variance to cover the violation.

1.7 Severability

- 1.7.1 The provisions of this chapter are hereby intended to be severable. If any of its sections, provisions, exceptions, or parts should be held unconstitutional or void, then the remainder of the ordinance will continue to be in full force and effect, it being the legislative intent now hereby declared that this ordinance would have been adopted even if such unconstitutional or void matter had not been included herein.

1.8 Effective date

- 1.8.1 This ordinance will take effect from and after the effective date of its passage and publication as required by law, the public welfare requiring it.

1.9 Repealer

- 1.9.1 All prior zoning ordinances and maps previously enacted are hereby repealed.

Municipality

ELEMENT 2 COMPREHENSIVE PLAN**2.1 Sector Plan**

- 2.1.1 The **municipal legislature** has adopted the sector plan as a part of the **comprehensive plan**.
- 2.1.2 The sector plan prescribes which community types are permitted as specified in **Table 2A. Community Unit Types by Sector**.

2.2 Sectors

- 2.2.1 Each sector guides whether a specific area is intended for growth or preservation.
- 2.2.2 Sector are intended to serve the following purposes:
 - a. **O1: consists of lands protected from future development;**
 - b. **O2: consists of lands of rural character where future development is very limited to not overburden resources or natural systems;**
 - c. **G1: consists of lands of primarily rural character where future development is limited to not overburden resources or natural systems;**
 - d. **G2: consists of lands that can support substantial growth by virtue of proximity to existing infrastructure and safety from hazards; and**
 - e. **G3: consists of land already developed that can support further and more intensified development.**

2.3 Community Unit Types

- 2.3.1 Community units are a regulatory category that defines the physical form, density, character, and extent of a new community.
- 2.3.2 Community units are used to structure the development of new communities on parcels **20** acres or greater.
- 2.3.3 Community unit types are as follows:
 - a. Hamlet, a small rural community oriented toward a common destination that consists of civic space;
 - b. Village, a rural community oriented toward a common destination that consists of civic space with small scale services and near a transportation route;
 - c. Medium intensity neighborhood (M-I-N), a small scaled, walkable neighborhood oriented toward a common destination that consists of civic space;
 - d. High intensity neighborhood (H-I-N), a highly intensive neighborhood that supports adjacent centers, oriented toward one or more common destinations, supporting its' own mixed use corridor, and near major transportation routes;

Policy Planning

This section may be use to regulate the municipality's comprehensive / general / master plan, empowering the transect-based SmartCode for land development, redevelopment, subdivision and zoning.

Comprehensive plan is used for consistency but must be calibrated to the local term.

This entire section is a modifier, depending on the type of Future Land Use Map (FLUM) the jurisdiction has adopted. To enable mixed use through most zones, the FLUM must be updated to reflect types of growth rather than types of use, as reflected in the uncalibrated sectors in 2.2.2.

COMMUNITY TYPES

Community types are determined through the visioning process at the policy level. They must be adjusted to reflect the local context. For example, municipalities without rural areas within their jurisdiction may have no hamlets or villages and small towns may have no major centers.

Both sectors and community types should be carefully calibrated for the local context.

- e. Minor center, a mixed use neighborhood with significant office, retail, and services, oriented towards a common destination or main street, and supported by adjacent neighborhoods; and
 - f. Major center, a very intense mixed use area focused on office, retail, and services, oriented toward a large mixed-use center or corridor and in a form of a commercial core on major transportation routes.
- 2.3.4 Community unit types are further limited by the prerequisite standards specified in [Table 2B. Community Unit Type Prerequisites](#).
- 2.3.5 New community units must have the characteristics specified in [Table 2C. Community Unit Type Requirements](#).

TABLE 2A. COMMUNITY UNIT TYPES BY SECTOR							
	Major Center	Minor Center	H-I-N	M-I-N	Town	Village	Hamlet
O1							
O2							■
G1							■
G2		■		■	■	■	■
G3	■	■	■	■	■		

TABLE 2B. COMMUNITY UNIT TYPE PREREQUISITES

	Wastewater Service	Transportation Adjacency	Community Adjacency
Major Center	Required	Adjacent to an arterial	Adjacent to existing or proposed development
Minor Center	Required	Within ¼ mile of an arterial	Within ¼ mile of existing development
High Intensity Neighborhood	Required	Within ¼ mile of an arterial	Adjacent to existing or proposed development
Medium Intensity Neighborhood	Required	Within ½ mile of a collector or arterial	Within ¼ mile of existing development
Town	Required	Adjacent to a rural route or highway	Not applicable
Village	Required	Within 1 mile of a rural route or highway	Not applicable
Hamlet	Recommended	Not applicable	Not applicable

TABLE 2C. COMMUNITY UNIT TYPE REQUIREMENTS

	Size	Main Civic Space Type	T2	T3	T4	T5	T6
Major Center	80 – 640 acres	Plaza or square				20 - 50%	50 - 80%
Minor Center	80 – 640 acres	Plaza, square or green			20 - 40%	60 - 80%	
High Intensity Neighborhood	80 – 200 acres	Plaza, square or green		0 - 20%	40 - 70%	20 - 50%	
Medium Intensity Neighborhood	80 – 200 acres	Square or green		20 - 50%	20 - 50%	10 - 30%	
Town	80 - 320 acres	Plaza, square or green	0 - 20%	30 - 50%	20 - 40%	10 - 30%	
Village	60 - 160 acres	Square or green	0 - 20%	40 - 60%	10 - 30%	10 - 30%	
Hamlet	20 - 60 acres	Green	0 - 20%	40 - 80%	0 - 40%	5 - 20%	

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Municipality

ELEMENT 3 LAND SUBDIVISION**3.1 Community Unit Standards**

- 3.1.1 Land subject to the requirements of this section must be configured in accordance with a single community unit or multiple adjacent community units.
- 3.1.2 Community unit size and composition requirements are specified in [Element 2 Comprehensive Plan](#).
- 3.1.3 Community unit proportions must be no greater than 3:1, measured by a circumscribed rectangle.
- 3.1.4 Community unit types are described in [§2.3.3](#).
- 3.1.5 Community unit types are further regulated in location as specified in [Table 2A. Community Unit Types by Sector](#) and [Table 2B. Community Unit Type Prerequisites](#).
- 3.1.6 Each community type must have the characteristics required by [Table 2C. Community Unit Type Requirements](#) for new community development. This table does not apply to existing communities.

3.2 Transportation Standards

- 3.2.1 [Community units within or adjacent to urbanized areas should be designated as a compact urban area type in coordination with the relevant transportation authorities.](#)
- 3.2.2 Right-of-way network
 - a. Rights-of-way are intended for use by pedestrians, bicyclists, public transportation vehicles, trucks and delivery vehicles, and personal vehicles to provide access to lots and civic space.
 - b. All rights-of-way must terminate at other rights-of-way, forming a network.
 - c. The spacing of rights-of-way is limited by the maximum block perimeter in accordance with [Section 3.3](#).
 - d. The [Consolidated Review Committee \(CRC\)](#) may permit cul-de-sacs where natural constraints limit the ability to terminate on other rights-of-way.
 - e. The right-of-way network should connect to properties outside of the application area and provide right-of-way stubs for connections to future development on adjacent undeveloped land.
 - f. Roadway assemblies must be designed in context with the urban form and mode share of the transect districts through which they pass. The specific design of rights-of-way is regulated by [Element 6 Right-of-way Regulations](#).

Transportation Context

This designation may be used in places that have suburban and compact urban standards, or where suburban standards are the normative sections. This will assure that Engineering and Public Works understand where the compact urban sections are applicable.

Consolidated Review Committee

If the local government has a single review committee in place, it should replace the CRC. See the duties of the CRC in [§7.1.5](#) to determine if this matches the local structure. If this body doesn't exist, it is recommended that one be established.

3.2.3 Pedestrian network

- a. Within T3, T4, T5, and T6 a continuous pedestrian network must provide access to all lots and civic space.

3.2.4 Bicycle network

- a. A bicycle network must be provided, connecting to existing bicycle networks, regional open space, existing trail networks, schools, and the main civic space of each community unit.
- b. The bicycle network may consist of dedicated and shared bicycle facilities specified in the transportation element as well as roadway assemblies with a design speed of 30 mph or less.

3.3 Block Standards

- 3.3.1 All developable land within the application area must be divided into blocks, except land in T1, T2, infrastructure or recreational corridors, and land directly abutting an interstate highway.

- 3.3.2 Block perimeters are to be defined by rights-of-way or pedestrian ways.

- 3.3.3 Block perimeters are limited to 3000 feet in length, except as follows:

- a. Special districts may be exempt in accordance with special district standards;
- b. Blocks containing more than 50% civic space are exempt;
- c. Blocks at the perimeter of the planning area may be exempt if connections cannot be made to adjacent properties;
- d. Blocks abutting County or State roadways where access is limited are exempt; and
- e. Blocks abutting natural waterways and slopes greater than 10% are exempt.

- 3.3.4 Blocks with a perimeter exceeding 2400 feet must include a pedestrian way traversing the block from the frontage of greatest length.

3.4 Zoning Standards

- 3.4.1 All developable land not assigned a special district must be assigned a transect district.

- 3.4.2 District boundaries are mapped to the **centerline of rights-of-way**.

- 3.4.3 Transitions between districts should occur at rear lot lines and side lot lines, they should not occur across rights-of-way.

District boundary locations

District boundaries may occur either along property lines or at the centerline of rights-of-way. Implications of this choice include district-related specifications for elements within the right-of-way and FAR & density calculations, if used. For the purpose of the SmartCode Framework, district boundaries are mapped to the centerline of rights-of-way.

Municipality

- 3.4.4 Transect districts must be assigned and mapped within planning areas according to the percentages specified in [Element 5 Zoning Regulations](#). Percentages are calculated independently for each community unit.

3.5 Platting Standards

- 3.5.1 All developable land must be subdivided into lots.
- 3.5.2 A minimum of 80% of lots must have at least one property line along a right-of-way.
- 3.5.3 Where applications include lots with property lines along more than one right-of-way, a primary frontage and secondary frontage must be specified.
- 3.5.4 Lot widths must meet the following width requirements.
- T2 – 200 ft. min., no maximum
 - T3 – 50 ft. min., no maximum
 - T4 – 18 ft. min., 84 ft. max.
 - T5 – 16 ft. min., 180 ft. max.
 - T6 – 22 ft. min., 300 ft. max.
- 3.5.5 Lots containing civic uses, and no other uses, may exceed maximum lot size requirements.

3.6 Civic Space Standards

- 3.6.1 Civic spaces permanently dedicated as public open space are required for each community unit.
- 3.6.2 A minimum of 5% of the developable land within each community unit must be assigned to civic space. No more than 20% of the developable land within each community unit may be assigned to civic space.
- 3.6.3 Civic spaces are permitted within transect districts according to [Table 5D. Use Matrix](#).
- 3.6.4 Each community unit must contain a main civic space located within 800 feet of the geographic center of the community unit unless topographic conditions, pre-existing right-of-way alignments or other circumstances prevent this location.
- 3.6.5 The main civic space type is limited as specified in [Table 2C. Community Unit Type Requirements](#).
- 3.6.6 One playground must be provided for every 40 acres of land within the community unit. Special districts may be exempt from this requirement.
- Playgrounds must be distributed throughout the planning area and may be located within other civic spaces.
- 3.6.7 The design, programming, and landscaping of all civic space must comply with [Table 3A. Civic Space Types](#).

Calibration

All metrics must be calibrated for the local context. For example, in towns that include zoning jurisdiction for agricultural lands, home sites may be restricted to a quarter section and the minimum lot width here would not comply. In cities with regional centers, lot widths may be correlated to block width and could be much larger. [§3.5.4](#) needs careful adjustment to reflect the context of the code.

Transitional districts

In many contexts transect districts in addition to the 6 districts included in this Framework are required. Transitional transect districts should be identified in one of the following formats:

District.Sub-District. For example the standard transect districts (T1, T2, T3, T4, T5, T6) could be supplemented with transitional transect districts, resulting in the following districts: T3.1, T4.1, T4.2, T5.1, T5.2, T5.3, T6.1, T6.2, T6.3, T6.4, T6.5.

Restricted, Limited, and Open (R,L,O). For example the standard transect districts (T1, T2, T3, T4, T5, T6) could be supplemented with transitional transect districts, resulting in the following districts: T1, T2, T3R, T3L, T3O, T4R, T4L, T4O, T5R, T5L, T5O, T6R, T6L, T6O.

When using transitional transect districts, add the appropriate sub-districts to [§3.5.4](#). Transitional transect districts provide a transition in intensity between districts.

3.6.8 Multiple community units

- a. Applications including two or more community units must include one park for every two community units.

3.7 Special Requirements

3.7.1 Applications may also include a Special Requirements map, assigning any of the following special requirements:

- a. A differentiation of the rights-of-way into an a-grid and b-grid.
 - i. Buildings along the a-grid are held to the highest standard of this [chapter](#) in support of pedestrian activity.
 - ii. Buildings along the b-grid may be more readily considered for waivers allowing automobile-oriented standards.
 - iii. The frontages assigned to the b-grid may not exceed [50%](#) of the total length of frontages within a community unit.
- b. Mandatory or recommended retail frontage, requiring or advising that a building provide a shopfront at sidewalk level along the entire length of its private frontage.
- c. Mandatory or recommended gallery frontage, requiring or advising that a building provide a permanent cover over the sidewalk, either cantilevered or supported by columns.
 - i. The gallery frontage designation may be combined with a retail frontage designation.
- d. Mandatory or recommended arcade frontage, requiring or advising that a building overlap the sidewalk such that the first floor facade is a colonnade.
- e. Mandatory or recommended porchfront, requiring or advising that a porch be included in the private frontage.
- f. Build-to line, requiring the placement of the building facade along the line.
- g. Mandatory or recommended terminated vista locations, requiring or advising a building be provided that responds visually to the location, as approved by the [CRC](#).
- h. Cross block passages, requiring that a minimum [8-foot-wide](#) pedestrian access be reserved between buildings.

Special Requirements

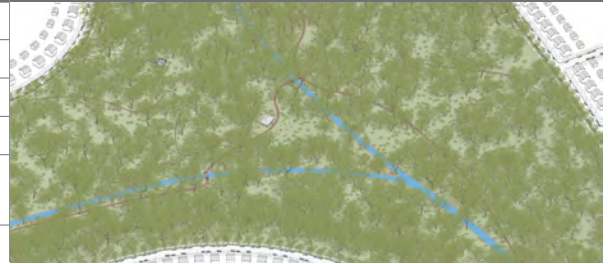
Special requirements are an additional tool for adding a level of refinement to a regulating plan. A-/b-grids relax the frontage requirements for some streets. This is particularly useful in areas that do not have an existing alley structure.

Mandatory frontage types and terminated vistas may be useful in historic districts, areas with strong design controls, or in private development.

TABLE 3A. CIVIC SPACE TYPES

Conservation Park

Transect district	T1
Size in acres	8 min.
Proportion	N/A
Edge condition	N/A
Surface	Planted, pervious paths and maintenance roads
Buildings	Pavilions and picnic areas

**Park**

Transect district	T2, T3, T4, T5, T6
Size in acres	5 min.
Proportion	1:5 max. in T5 and T6
Edge condition	Rights-of-way required on all sides in T5 and T6
Surface	Planted, pervious and non-pervious paths, paved gathering spaces, sports facilities
Buildings	Pavilions, picnic areas, storage, rest rooms, food service

**Green**

Transect district	T3, T4, T5
Size in acres	1/2 min., 5 max.
Proportion	N/A
Edge condition	Right-of-way required on at least one side
Surface	Planted, paved 15% max., pervious paving recommended
Landscape	40% mature canopy required

**Square**

Transect district	T4, T5, T6
Size in acres	1/4 min., 4 max.
Proportion	1:5 max.
Edge condition	Rights-of-way required on two sides, min.
Surface	Paved 50% max., pervious paving recommended
Landscape	30% mature canopy required



TABLE 3A. CIVIC SPACE TYPES

Plaza

Transect district	T5, T6
Size in acres	1/8 min., 3 max.
Proportion	1:4 max.
Edge condition	Rights-of-way required on two sides, min.
Surface	Paved 50% min., pervious paving recommended
Landscape	20% mature canopy required
Buildings	Pavilions, storage, rest rooms, food service, vending



Neighborhood Multipurpose Field

Transect district	T3, T4, T5
Size in acres	3 min., 10 max.
Proportion	1:5 max.
Edge condition	Rights-of-way required on two sides, min.
Surface	Planted, sports fields and courts, paved 20% max.
Landscape	20 foot landscape zone at perimeter, canopy trees required. Fields may serve secondary purpose as stormwater management.



Greenway

Transect district	T3, T4, T5, T6
Size in feet	50 min. in width, 1,000 min. in length
Proportion	N/A
Edge condition	Rights-of-way or pedestrian way required on one side, min.
Surface	Lawn, paved 20% max.
Landscape	20% mature canopy required



TABLE 3A. CIVIC SPACE TYPES

Pocket Park

Transect district	T3, T4, T5, T6
Size in square feet	2,000 min., 8,000 max.
Proportion	1:5 max.
Edge condition	Rights-of-way or pedestrian way required on one side, min.
Surface	Planted, play equipment, paved 30% max.
Landscape	40% mature canopy required

**Community Garden**

Transect district	T3, T4, T5
Size in acres	N/A
Proportion	N/A
Edge condition	N/A
Special requirement	Water supply required
Landscape	75% noon-time solar exposure

**Pedestrian Passage**

Transect district	T3, T4, T5, T6
Size in acres	N/A
Width	8 ft. min. in T3 and T4, 20 ft. min. in T5 and T6
Surface	Continuous paved walkway required, seating and gathering places encouraged, planted opportunistically



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ELEMENT 4 SPECIAL AREA REGULATIONS**4.1 General**

- 4.1.1 Some development patterns do not fit within transect districts or need to be modified for unique situations.

4.2 Special Districts

- 4.2.1 Areas that, by their intrinsic size, function, or configuration, cannot conform to the requirements of any transect district or combination of zones are designated as special districts.
- 4.2.2 Special districts may be mapped within or outside of community units. If mapped within a community unit, a special district may not exceed 20% of the net community unit area.
- 4.2.3 Business special district (SDB)
- SDB is available for industrial and manufacturing areas, and areas with a high concentration of office and light industrial uses.
 - SDB is limited to a maximum size of 120 acres.
 - SDB may be mapped within or adjacent to Town, High Intensity Neighborhood, Minor Center, and Major Center community units.
 - SDB must comply with the specifications of [Table 4D. Business Special District Standards](#).

4.3 Transit Overlay Districts (TOD)

- 4.3.1 Transit Overlay Districts apply to areas around all transit stops of the following types:
- Regional Rail, high-speed rail transit with infrequent stops, typically more than 10 miles apart, providing access to major destinations and city centers within and outside of the local region.
 - Commuter Rail, moderate-speed rail transit with regular stops, typically less than 5 miles apart, providing access to major and minor employment and population centers.
 - Light Rail & Bus Rapid Transit (BRT): Light Rail, low-to-moderate-speed rail transit with frequent stops, typically spaced one mile or less apart, providing access along mixed-use corridors, and to employment and population centers. Light Rail travels along a dedicated track, separate from other vehicles. Bus Rapid Transit, low-to-moderate-speed bus transit with frequent stops, typically spaced one mile or less apart, providing access along mixed-use corridors, and to employment and population centers. Bus Rapid Transit travels within dedicated lanes, separate from other vehicles.

Special Designations

There are some situations for which the basic transect districts will require modification or some where they require replacement.

Special Districts

Special districts are large areas, necessary within the city, that cannot comply with block size, mixed use, or some other standard common to the multi-modal human habitat. One sample is provided here, but others that may be necessary include industrial, airports, hospitals, universities, or large government campuses.

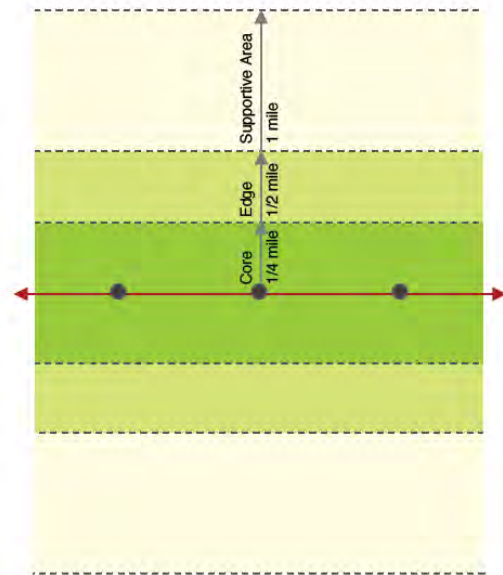
Historic districts overlay

Historic districts also vary dramatically from place to place. Style is not addressed within zoning, but additional restrictions relative to massing, rhythm, materials, and demolition procedures may be added through a historic district overlay. It is not included within the model due to the variety in historic places.

Transit overlay districts

TOD varies by type and location, and must be carefully addressed at the local level. However, common adjustments made for TOD include parking requirements and building height. If the code addresses density through du/acre, this will also need to be adjusted upward.

- 4.3.2 Transit Overlay Districts do not apply to Streetcars, Trolleys, and standard frequency Bus service.
- 4.3.3 Transit Overlay Districts are divided into the following subdistricts.
- a. Transit Core: An area within very close proximity to the transit stop, 1/4 mile, where the majority of users will walk to access all transit systems.
 - b. Transit Edge: An area within relatively close proximity to the transit stop, between 1/4 and 1/2 mile, where a high percentage of users will walk or bike to access most transit systems.
 - c. Transit Supportive Area: An area where some users will walk or bike to access moderate and long-distance transit systems where local bus service does not provide a viable alternative.
- 4.3.4 District boundaries are determined by the distance that 50% or more of a lot is located from a transit stop or station as illustrated in [Table 4A. TOD Subdistricts](#) and as follows:
- a. Within 1/4 mile, the lot is within the Transit Core;
 - b. Within 1/2 mile, the lot is within the Transit Edge; and
 - c. Within 1 mile, the lot is within the Transit Supportive Area.
- 4.3.5 Maximum building height is adjusted upwards for sites within Transit Overlay Districts according to [Table 4B. TOD Height Adjustment](#).
- 4.3.6 Minimum required parking is adjusted downward for sites within Transit Overlay Districts according to [Table 4C. TOD Parking Adjustment](#).

TABLE 4A. TOD SUBDISTRICTS**Station Area Transit Sheds: Centered on Station****Station Area Transit Sheds: Transit Corridor****TABLE 4B. TOD HEIGHT ADJUSTMENT**

TOD Service	Subdistrict	T3	T4	T5	T6
Regional Rail & Commuter Rail	Transit Core		+1 story	+2 stories	+4 stories
	Transit Edge			+1 story	+2 stories
	Supportive Area				+1 story
Light Rail & Bus Rapid Transit	Transit Core		+1 story	+1 sotry	+2 stories
	Transit Edge				+1 story
	Supportive Area				

TABLE 4C. TOD PARKING ADJUSTMENT

TOD Service	Subdistrict	Reduction
Regional Rail	Transit Core	No minimum required
	Transit Edge	No minimum required
	Supportive Area	25% reduction
Commuter Rail	Transit Core	No minimum required
	Transit Edge	No minimum required
	Supportive Area	25% reduction
Light Rail	Transit Core	50% reduction
	Transit Edge	50% reduction
	Supportive Area	25% reduction
Bus Rapid Transit	Transit Core	25% reduction

4.4 Pink Zone Overlay District (PZ)**4.4.1 Intent**

- a. To enable and encourage incremental and ongoing development,
- b. To allow more people to participate in the building of their homes, businesses, and communities,
- c. To do so by removing barriers to small-scale economic development, ensuring that community-building requires less time, fewer resources for compliance, and is more streamlined for entrepreneurs and community developers by providing ways to navigate the regulatory and bureaucratic processes.

4.4.2 Area

- a. The area designated pink zone overlay district (PZ) is hereby added to the [municipal](#) zoning map and designated “pink zone overlay district”.

4.4.3 Applicability

- a. PZ regulations apply to all buildings and properties within the mapped area.

4.4.4 Conflict

- a. Where the PZ and the underlying zoning district are in conflict, the overlay district standards apply.

4.4.5 Subdivision standard adjustments

- a. Lot size minimums do not apply.

4.4.6 Open space requirements

- a. Open space requirements of the underlying zoning district do not apply.

4.4.7 Access management

- a. Alley access must be used where it is available. If alley access is not available, the street frontage with the lowest average car count must be used.

4.4.8 Prohibited uses

- a. The following uses are prohibited in the pink zone overlay district:
 - i. Pawn shops;
 - ii. Bail bond services;
 - iii. Maintenance and repair services;
 - iv. Gasoline or fuel sales; and
 - v. Building material sales and services.

Pink Zone Overlay

A Pink Zone overlay is an area where the red tape is lightened, and it identifies an area where new protocols are pre-negotiated and experiments are conducted, all with the goal of removing impediments to economic development and community-building.

A Pink Zone has some similarities to innovation districts, metropolitan redevelopment areas and other tools used to spur revitalization, engage populations that have been left out of the development process, stimulate sustainable economic activity and build assets through incubator and maker spaces. Pink Zones address jobs and housing, and encourage community-supportive enterprises such as Main Street businesses and start-up makers and platforms such as shared work spaces, kitchens, community hubs, and markets.

Municipality

4.4.9 Temporary uses

- a. Temporary uses are permitted without the requirement for a permanent use permit, and any use permitted by the underlying zoning is permitted on a temporary basis. Temporary uses may include tent retail, food trucks, vending trailers, and micro-retail. Temporary uses do not require parking.

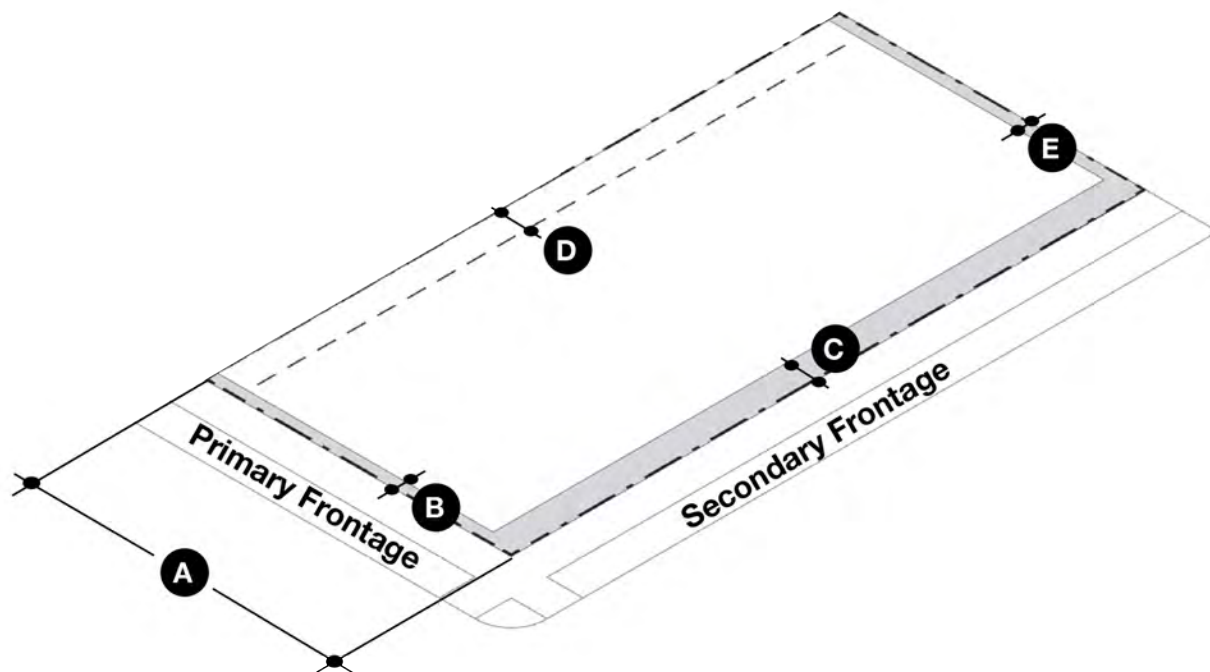
4.4.10 Setbacks

- a. All parcels may be zero lot line at front and side.
- b. If detached, buildings must meet underlying zone setbacks.

4.4.11 Lighting

- a. All lighting must be directed downward and shielded so that the light source is not visible from off-site.

TABLE 4D. BUSINESS SPECIAL DISTRICT STANDARDS



Lot Occupation		Accessory Structures	
A Lot Width	70 ft. min.	Front Setback	B + 20 ft. min.
Lot Coverage	70% max.	Sidestreet Setback	2 ft. min.
Buildings		Side Setback	0 ft. or 5 ft. min.
B Front Setback	2 ft. – 20 ft.	Rear Setback	5 ft. min.
C Sidestreet Setback	2 ft. min.	Rear Alley Setback	15 ft. min. from centerline
D Side Setback	0 ft. or 6 ft. min.	Height	3 stories max.
E Rear Setback	5 ft. min.	Parking	
E Rear Alley Setback	3 ft. min.	Front Setback	20 ft. min.
Height	6 stories max.	Sidestreet Setback	3 ft. min.
Ground Floor Glazing	30% min.	Structure Height	60 ft. max.
Upper Floor Glazing	15% min.		
Entry Frequency	100 ft. max.		

ELEMENT 5 ZONING REGULATIONS**5.1 Buildings and Yards****5.1.1 Building Placement**

- a. Structures must be set back from lot boundaries as specified in [Table 5L – Table 5P](#), and as follows:
 - i. Front specifies the setback from the lot line associated with the address;
 - ii. Side street specifies the setback from any lot line abutting a street other than the front lot line;
 - iii. In T5 and T4, where there are multiple structures on one lot, the side street maximum setback applies to only the nearest structure;
 - iv. Side specifies the setback from side lot lines other than those qualifying for a side street setback;
 - v. Rear specifies the setback from the rear lot line, except where abutting an alley; and
 - vi. Rear Alley specifies the setback from the rear lot line in instances that it abuts an alley.
- b. Elements that project from facades are permitted to encroach into setbacks as specified in [§5.2.7](#).
- c. In T5 and T6, buildings must occupy **70%** or more of the lot width within the front setback, excluding the width of required side and side street setbacks.
- d. Utility easements may cause buildings to exceed maximum setbacks and may require additional setbacks.
- e. Buildings and covered structures are limited in the total area they may occupy as a percentage of the net lot area as specified in [Table 5L – Table 5P](#).

5.1.2 Building Height

- a. Building height is limited according to [Table 5L – Table 5P](#), measured as follows:
 - i. Building height is measured in stories above sidewalk grade adjacent to the principal building entrance;
 - ii. Stories are measured from finished floor to finished ceiling;
 - iii. Stories are limited to **14 feet** high;
 - iv. Stories exceeding **14 feet** are counted as one story for every **14 feet**;

Development Intensity

Development intensity may be controlled by a single method or multiple methods in coordination. Consult with a zoning attorney regarding which tools are appropriate for each situation. For the purpose of the SmartCode Framework bulk standards in combination with parking restrictions are used.

Bulk Standards – Bulk standards restrict maximum building bulk through building setbacks, building height, and lot coverage. Bulk standards are integral to any Form Based Code. They enforce an easy to understand set of universal per-district restrictions. Unlike FAR, FLR, and Dwelling Units, bulk standards do not vary according to lot area or whether calculations are based upon gross or net lot area.

Parking Restrictions – Parking restrictions are typically used along with other tools. If utilizing parking restrictions alone, a maximum number of off-site parking spaces permitted may be required. Parking restrictions limit the square footage of non-residential uses and the number of residential units per building site by requiring adequate parking is provided. Areas with extensive transit services often exempt or heavily reduce parking restrictions.

Floor Area Ratio – Floor area ratio (FAR) may be used to restrict the total volume of conditioned or interior space of a building, regardless of its use. FAR is calculated as a multiplier of the square footage of lot area, gross or net. It typically excludes parking structures from built area calculations which can result in building heights far exceeding expectations. To address this, see floor lot area (FLR). FAR is often used in combination with bulk standards, in which cases care must be taken to ensure the two are not contradictory.

Floor Lot Ratio – Floor lot ratio (FLR) may be used to restrict the total volume of built area, regardless of its use. FLR is calculated as a multiplier of the square footage of lot area, gross or net. Unlike FAR, FLR includes parking structures and covered spaces in the built area calculations.

- v. For non-residential and mixed-uses:
 - (a) Ground floor stories must be 11 feet in height or greater;
 - (b) Ground floor stories may be up to 25 feet;
 - (c) Ground floors exceeding 25 feet in height are counted as one story for every 14 feet; and
 - (d) Mezzanines extending beyond 30% of the ground floor area are counted as an additional story;
- vi. Below ground stories do not count toward building height provided they do not extend more than 4 feet above sidewalk grade; and
- vii. Height limits do not apply to attics, masts, belfries, clock towers, chimney flues, water tanks, or elevator bulkheads.
- b. Parking structure height is limited according to Table 5L – Table 5P, measured as follows:
 - i. Parking structure height is measured in feet above average adjacent grade; and
 - ii. Parking structures lined along 80% or more of street facing elevations are limited in height to the eave or parapet of lining buildings.

Dwelling Units – Dwelling units per acre (du/ac) may be used to restrict the size of buildings based upon the lot area. Du/ac calculations align more directly with regional planning goals but require non-residential units be calculated as a ratio of square feet to dwelling units.

5.2 Frontage Requirements

5.2.1 Frontage requirements regulate the following:

- a. Building facades nearest the front and side street lot lines.
- b. Elements that project from building facade into front and side street setbacks such as porches, terraces, stoops, awnings, canopies, and bay windows.
- c. Landscape elements between front and side street lot lines and building facades nearest those lot lines.

5.2.2 A frontage type must be specified for each building facade facing a street as follows:

- a. Frontage types are specified as a combination between a frontage yard and frontage elements along the facade;
- b. Frontage yards must be configured according to Table 5A. Frontage Yards;
- c. Frontage elements must be configured according to Table 5B. Frontage Elements;
- d. Multiple frontage types may be used sequentially along a facade; and
- e. A shopfront frontage is required for all ground floor retail uses.

Frontage Types

Frontage types vary significantly by location, climate and culture. The SmartCode Framework includes a collection of very common frontage types with minimal regulations related to frontage type. Frontage types can be renamed, removed and added for local context. For example, arcade frontages are commonly removed where municipalities do not allow habitable space above the public right-of-way.

The simplest standards would include the requirement of a certain percentage of clear glass and the primary entry must face a street. This might be all a small town is able to administer. Cities that have a design review board or city architect may include a requirements for the composition of a shopfront, or proportions for galleries.

Municipality

- 5.2.3 Buildings located within 30 feet of streets and civic spaces must comply with the following:
- Building entries must be provided from streets and civic spaces at the minimum frequency specified in [Table 5L – Table 5P](#);
 - Buildings on corner lots are exempt from entry frequency requirements along the shorter facade if it is less than **75 feet** in length; and
 - Entries may be recessed from the facade up to 8 feet in depth.
- 5.2.4 Facades must meet the minimum clear glass requirements of [Table 5L – Table 5P](#) and as follows:
- Glass percentage is calculated individually for each facade and is measured between two and ten feet in height above grade along the length of the facade;
 - The entire frame and structure of doors, windows, and storefront systems are considered glass for this calculation;
 - Shopfronts must have a minimum of **70%** clear glass; and
 - Tinted, mirrored and reflective glass, and glass covered by screening sheets, white, or UV protection film are prohibited.
- 5.2.5 Blank walls visible from the public sidewalk must not exceed **50 linear feet**. Walls along interior side lot lines with a zero setback are exempt from this requirement.
- 5.2.6 Where building facades do not occupy the entire frontage length in T5, a streetscreen is required to occupy the remaining frontage as follows:
- Streetscreens must be between 4 and 8 feet in height;
 - Openings in the streetscreen for vehicular access may be no wider than **30 feet**; and
 - Streetscreens must be opaque or include planting towards the lot interior to increase opacity.
- 5.2.7 Encroachments.
- Minor facade elements may encroach into setbacks as follows:
 - Roof overhangs, cornices, window and door surrounds, and other facade decorations may encroach into setbacks up to 2 feet beyond the structure they are attached to; and
 - Minor facade elements must not encroach into rights-of-way.

- b. Major facade elements may encroach into setbacks as follows:
 - i. Major facade elements may encroach according to frontage type as specified in [Table 5A. Frontage Yards](#) and [Table 5B. Frontage Elements](#); and
 - ii. Major facade elements include bay windows, bow windows, display windows, balconies, stoops, porches, awnings, galleries, and arcades.
 - c. Ground floor retail uses may utilize the public sidewalk for seating, serving, displays of merchandise, and other business related activities provided a 5 foot clear pedestrian path is maintained.
- 5.2.8 All outdoor electrical, plumbing, and mechanical equipment must be located behind the front facade or concealed from street view with a screen or wall. These facilities may not encroach into any setback.
- 5.2.9 Drive-through facilities and lanes must be located behind the building relative to sidewalks.
- a. Queuing lanes must not interfere with pedestrian circulation.
 - b. Queuing lanes may not be located in front of the building.
 - c. Drive-through canopies and other structures, where present, must be constructed from the same materials as the building, and with the same level of architectural quality and detailing.
- 5.2.10 Shopfront frontages not combined with other frontage types may include awnings as follows:
- a. Awnings must be fabric;
 - b. Awnings may be movable;
 - c. Awnings must extend a minimum of **6 feet** from the building facade;
 - d. Awnings must be set back from the curb a minimum of 2 feet;
 - e. A minimum of **10 foot** clearance must be maintained above the sidewalk; and
 - f. **Awnings must span the shopfront area up to 80% of its width without gaps, except between tenants.**

TABLE 5A. FRONTAGE YARDS

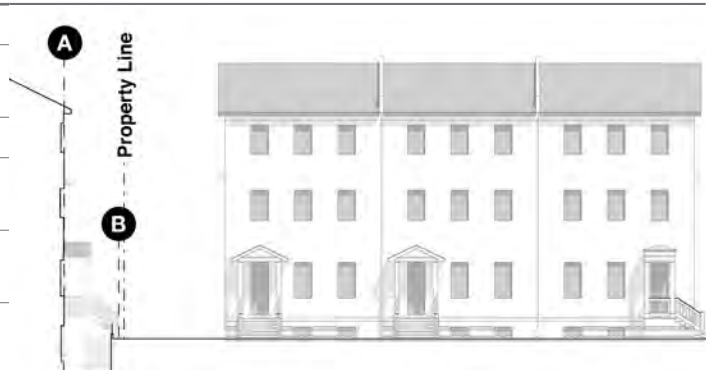
No Yard

Transect District	T6, T5
Elements	Arcade, gallery, urban terrace, forecourt, shopfront
A Max. Setback	N/A
B Encroachment	Elements up to 100% of setback; may extend into ROW by permit
Surface	Paved, unit paver or to match sidewalk
Fencing	None



Urban Yard

Transect District	T5, T4
Elements	Stoop, porch, urban terrace, light well
A Max. Setback	8 ft.
B Encroachment	Elements up to 100% of setback
Surface	Paved, raised planter, landscaped optional in T4
Fencing	Permitted where landscaped in T4



Shallow Yard

Transect District	T4, T3
Elements	Stoop, porch, terrace, light well
A Max. Setback	18 ft.
B Encroachment	Elements up to the greater of 8 ft. or 60% of the setback
Surface	Landscaped
Fencing	Optional



Common Yard

Transect District	T4, T3
Elements	Stoop, porch, terrace
A Min. Setback	18 ft.
B Encroachment	Elements up to the greater of 8 ft. or 40% of the setback
Surface	Landscaped
Fencing	None

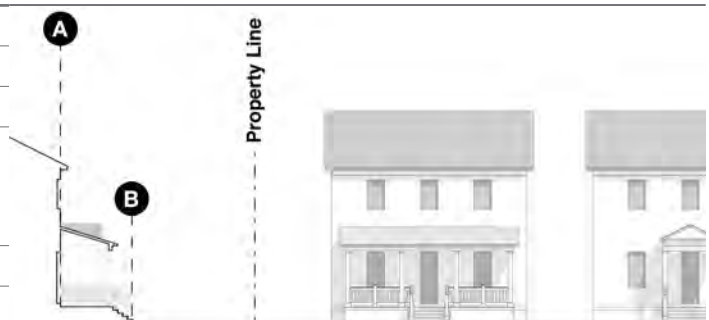
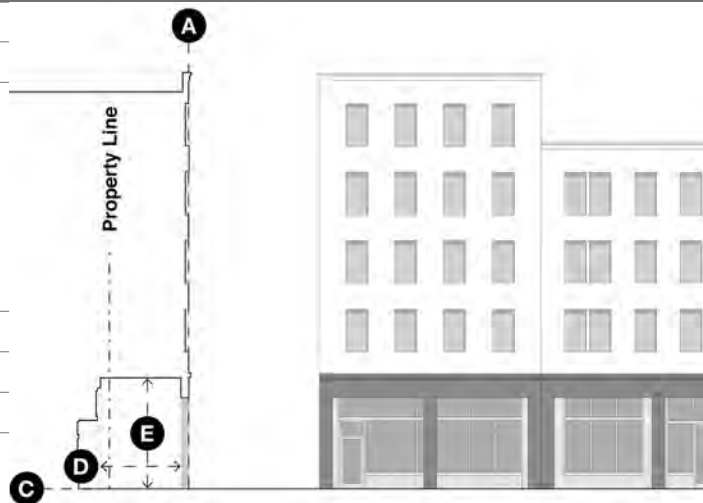


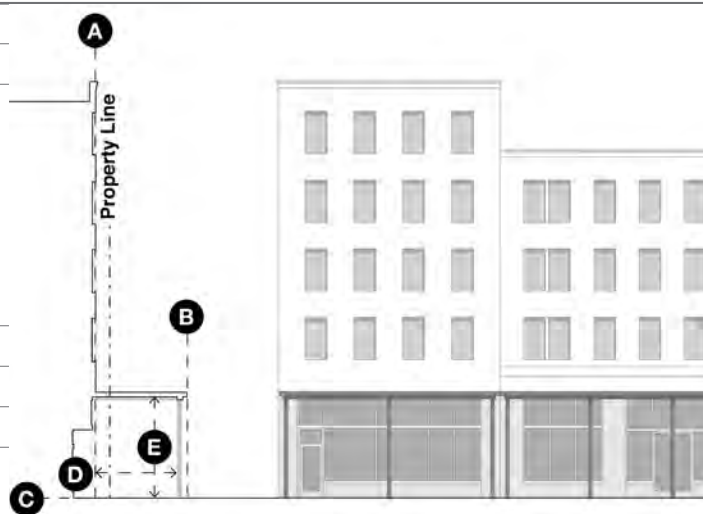
TABLE 5B. FRONTAGE ELEMENTS

Arcade

Combinations	Shopfront, common entry
A Max. setback	N/A
B Encroachment	Building and arcade to within 2 feet of the curb; outdoor display, bay and display windows 100% of the setback; seating by permit
C Entry condition	By combination
D Horizontal clear	8 ft. min.
E Vertical clear	10 ft. min.
Special Conditions	Utility coordination required

**Gallery**

Combinations	Shopfront, common entry
A Max. Setback	6 ft.
B Encroachment	Gallery to within 2 feet of the curb; outdoor display, bay and display windows 100% of the setback; seating by permit
C Entry Condition	By combination
D Horizontal Clear.	8 ft. min.
E Vertical Clear.	10 ft. min.
Special Conditions	Utility coordination required

**Urban Terrace**

Combinations	Shopfront, awning, canopy
A Min. Setback	10 ft.
B Encroachment	Terrace and seating 100% of setback; outdoor display, bay and display windows up to 4 ft.
C Entry Condition	At grade entry
D Horizontal Clear.	N/A
E Vertical Clear.	N/A
Special Conditions	Wall permitted along frontage to 42 in. high

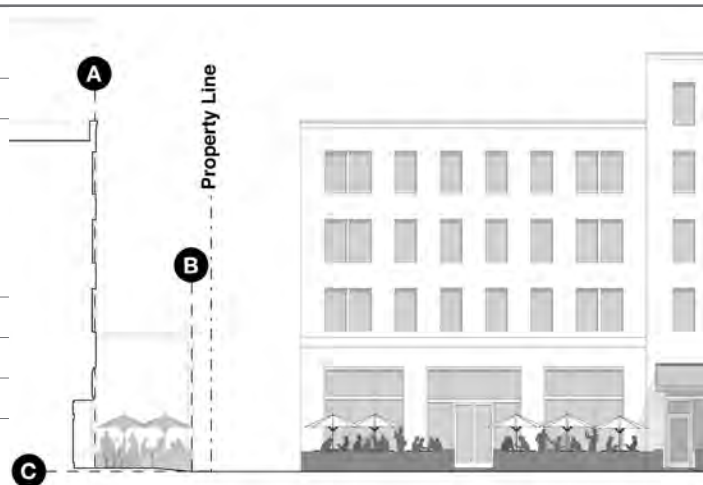
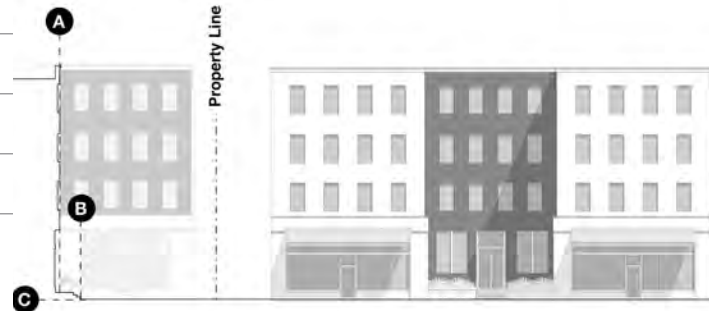


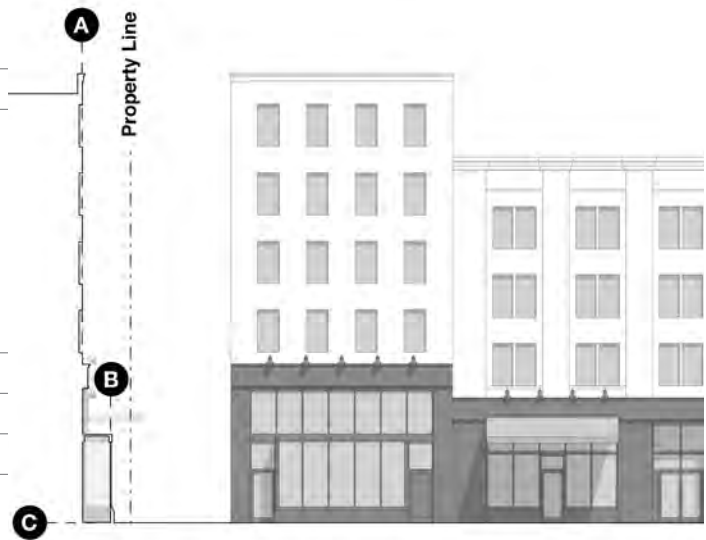
TABLE 5B. FRONTAGE ELEMENTS

Forecourt

Combinations	Arcade, gallery, urban terrace, shopfront, common entry
A Max. Setback	60 ft.
B Encroachment	By combination
C Entry Condition	By combination
Special Conditions	Courtyard 400 - 1,600 sq.ft., maximum 40% of frontage buildout width

**Shopfront**

Combinations	Arcade, gallery, urban terrace, forecourt
A Max. Setback	By district
B Encroachment	Seating by permit; bay and display windows to lot line; awning and canopy by element
C Entry Condition	At grade entry
D Horizontal Clear.	N/A
E Vertical Clear.	N/A
Special Conditions	Band, window, or blade sign required

**Awning or Canopy**

Combinations	Shopfront, urban terrace, forecourt, common entry
A Max. Setback	N/A
B Encroachment	Awning or canopy to within 2 ft. of curbs
C Entry Condition	N/A
D Horizontal Clear.	8 ft. min.
E Vertical Clear.	8 ft. min.
Special Conditions	Awning must be fabric and exclude side panels.

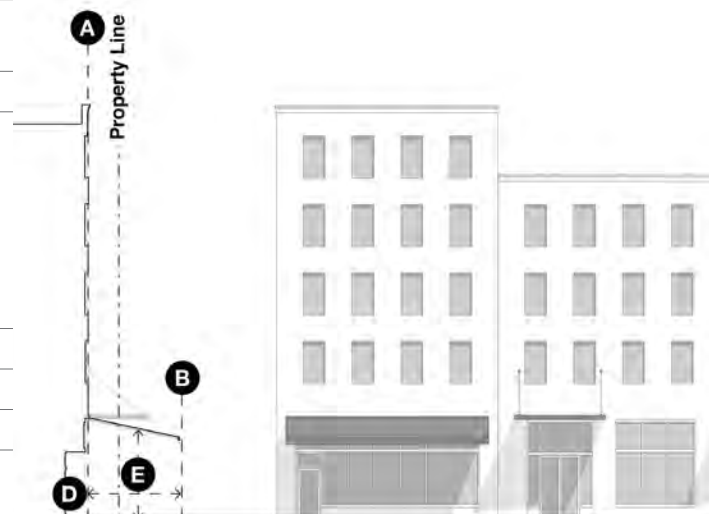


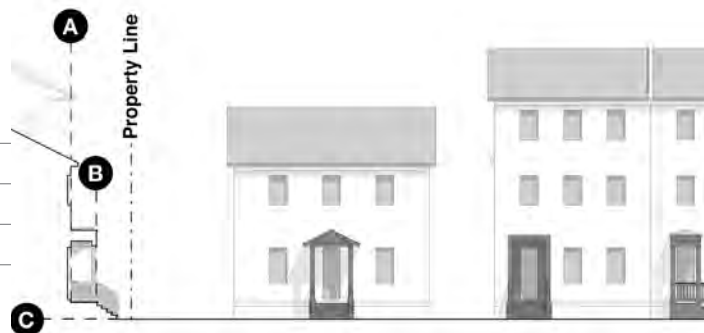
TABLE 5B. FRONTAGE ELEMENTS

Terrace

Combinations	None
A Min. Setback	10 ft.
B Encroachment	Terrace the greater of 8 ft. or 40% of setback
C Entry Condition	Raised entry
D Horizontal Clear.	N/A
E Vertical Clear.	N/A
Special Conditions	Wall or fence permitted along the terrace edge; 400 sq.ft. maximum area

**Stoop**

Combinations	Light well, terrace
A Min. Setback	3 ft.
B Encroachment	Landing, cover, and stairs up to 100% of setback in T4, T5, and T6, and up to 10ft. in T3 and T2
C Entry Condition	Raised entry
D Horizontal Clear.	Landing 5 ft. min.
E Vertical Clear.	Covered stoop 8 ft. min.
Special Conditions	Stoop may be engaged into the building volume.

**Porch**

Combinations	None
A Min. Setback	10 ft.
B Encroachment	Porch up to the greater of 8 ft. or 60% of the setback
C Entry Condition	Raised entry
D Horizontal Clear.	Porch 8 ft. min.
E Vertical Clear.	Porch 8 ft. min.
Special Conditions	None



Municipality

5.3 Use

5.3.1 Uses are limited according to [Table 5D. Use Matrix](#).

5.3.2 Additional restrictions in [Table 5E. Use Restrictions](#) apply to commercial uses.

TABLE 5D. USE MATRIX

	T2	T3	T4	T5	T6
Agriculture					
Agricultural processing, packaging, and warehousing	■				
Auction yard	■				
Commercial dairy, poultry, and swine production					
Feed lot					
Farming and ranching	■				
Grain storage	■				
Greenhouse	■				
Stables	■	■			
Stockyard	■				
Wine tasting	■			■	■
Civic Space					
Community garden		■	■	■	
Green		■	■		
Neighborhood multipurpose field		■	■		
Park	■	■	■		
Playground		■	■	■	■
Plaza				■	■
Square			■	■	■
Commercial					
Adult retail and services					
Automobile sales (no vehicles outdoors)				■	■
Automobile service				■	■
Commercial laundry				■	■
Crematorium					
Day care		■	■	■	■
Dry cleaners				■	■
Food and beverage			■	■	■
Funeral home				■	■
Gas station				■	■
Hospital				■	■
Market, permanent				■	■
Market, temporary	■			■	■
Office			■	■	■
Package liquor store				■	■
Retail			■	■	■
Self-storage					
Service, professional				■	■
Service, personal			■	■	■

Conditional and Special Uses

Conditional and special uses are explicit to the local context and not included within the model SmartCode. However, if they're needed locally, they should have clear and objective standards or conditions for the use, rather than being subject to negotiation, which is commonly the case.

TABLE 5D. USE MATRIX					
	T2	T3	T4	T5	T6
Entertainment					
Adult entertainment					
Indoor sports facility				■	■
Outdoor sports facility					
Theater, excluding drive-in			■	■	■
Industrial and Manufacturing					
Ground transportation terminal					
Industrial, heavy					
Industrial, light					
Manufacturing, heavy					
Manufacturing, light (artisanal)	■			■	
Warehouse storage and distribution					
Institutional					
Convention or exhibition facilities				■	■
Cultural (library, museum, gallery)			■	■	■
Performing Arts				■	■
Meeting Hall	■		■	■	■
Religious assembly	■	■	■	■	■
School: elementary or middle		■	■	■	■
School: high			■	■	■
School: college or university				■	■
Lodging					
6 rooms or less	■		■	■	
12 rooms or less				■	
More than 12 rooms				■	■
Residential					
Home occupation	■	■	■	■	■
1 dwelling unit per lot	■	■	■		
1 dwelling unit per lot (zero lot line)			■	■	
2 dwelling units per lot	■	■	■	■	
4 dwelling units per lot	■		■	■	
8 dwelling units per lot			■	■	■
More than 8 dwelling units per lot				■	■

■ Permitted

Municipality

5.4 Specific Use Restrictions

5.4.1 General Restrictions

- a. Uses are further restricted as specified in [Table 5E. Use Restrictions](#).

TABLE 5E. USE RESTRICTIONS					
	T2	T3	T4	T5	T6
Commercial Uses	N/A	2,000 sq. ft. max.	2,000 sq. ft. max.	60,000 sq. ft. max.	60,000 sq. ft. max.

5.4.2 Home Occupations

- a. Home occupations are divided into two categories, as follows:
- Type A home occupations involve only the residents of the household; and
 - Type B home occupations involve employees who do not reside on the premises and customers who visit the site.
- b. Type A home occupations are limited as follows:
- Must not exceed 25% of the dwelling unit or 800 square feet, whichever is less;
 - Customers and employees visiting the home are not allowed; and
 - Business signage is prohibited.
- c. Type B home occupations are limited as follows:
- Prohibited in T3;
 - Hours of operation are limited to 7:00 AM through 7:00 PM;
 - Must not exceed 40% of the dwelling unit or 1,000 square feet, whichever is less;
 - A maximum of 3 individuals not residing in the household, customers or employees, may visit the site at one time, except adult or child day care homes; and
 - Business signage is limited to one non-illuminated sign, no greater than 3 feet in area.
- d. Prohibited uses in home occupations:
- Repair or assembly of vehicles, equipment, and large appliances;
 - Dispatch facilities;
 - Labor pools;
 - Employment agencies;
 - Warehousing;

Use Restrictions

This model table is designed to restrict large format retail in the urban context. Other use restrictions may be added if nuance is needed for the use matrix.

- vi. Animal sales or kennels;
- vii. Merchandise warehousing, packing, and shipping is prohibited in Type A home occupations; and
- viii. Retail sales are restricted to drop-shipment only.
- e. Nuisances
 - i. The dwelling and site must remain residential in appearance.
 - ii. No equipment, material, or process may be used that creates noise, vibration, glare, smoke, fumes, particulate matter, excessive heat or humidity, electrical interference, odors, or hazards.
 - iii. Explosives, highly flammable materials, and toxic or hazardous waste is prohibited.
 - iv. Outdoor equipment not typically found or used for domestic household use is prohibited.
 - v. Residential trash and recycling volumes and facilities may not be materially exceeded.
 - vi. Commercial vehicles are prohibited unless stored within a garage.

5.4.3 Accessory dwelling units.

- a. One accessory dwelling unit per principal dwelling unit is permitted in all districts.
- b. Accessory dwelling units in T3 and T4 may not exceed 720 square feet per story.
- c. Accessory dwelling units may be attached or detached.
- d. Accessory dwelling units are limited to one bedroom.

5.5 Parking Standards

5.5.1 Bicycle Parking

- a. Bicycle parking is required as specified in [Table 5F. Bicycle Parking Requirements](#) and as follows:
 - i. A minimum percentage of spaces may be required to be enclosed by a structure or lockable fence for parking that may exceed 4 hours; and
 - ii. Enclosed spaces for multi-family residential must be located at ground level.
- b. Bicycle racks provided within the front setback or within the public right-of-way must be configured as specified in the [Element 6 Right-of-way Regulations](#).

Municipality

5.5.2 Automobile Parking

- a. Automobile parking is required in the amount specified in [Table 5H. Automobile Parking Requirements](#) and as follows:
 - i. All uses within the application are added to determine the minimum required parking;
 - ii. When requirements result in a fractional number, fractions are rounded down;
 - iii. Uses within liner buildings less than 30 feet in depth and no more than two stories are exempt from required parking; and
 - iv. An applicant may elect to commission a parking study, providing evidence of parking requirements below the standards of this [chapter](#).
- b. Required parking may be adjusted downward by shared parking according to [Table 5I. Parking Occupancy Rate Table](#) and as follows:
 - i. The applicant must submit a parking demand summary sheet showing the process for calculating the reduction as follows:
 - (a) The gross minimum number of parking spaces is multiplied by the “occupancy rate” in [Table 5I. Parking Occupancy Rate Table](#), for each use for the time periods respectively;
 - (b) The gross minimum numbers of parking spaces for each of the purposes referred to for each time period will be added to produce the aggregate gross minimum numbers of parking spaces for each time period; and
 - (c) The greatest of the aggregate gross minimum numbers of parking spaces for each period is determined.
- c. Required parking may be adjusted downwards where the following provisions for cyclists are provided:
 - i. 1 automobile parking space may be reduced for every 4 bicycle parking spaces provided in excess of that required; and
 - ii. 1 automobile parking space may be reduced for every shower provided for non-residential uses, not to exceed a reduction of 10 spaces.
- d. Required parking for residential uses may be adjusted downwards by 50% where spaces serve state licensed assisted living and senior independent living facilities.

Parking Standards

Parking goals and requirements vary by location within the region, intensity of the subject area, local driving rates, and the progressiveness of the municipality. Any of the following methods may be used individually or in combination: minimum required parking, maximum provided parking, shared parking, and no requirements. These may be further modified by TOD and reductions for alternative energy vehicles, compact vehicles, provisions for cyclists, parking area restriction systems, and historic downtown exclusions. For the purpose of the SmartCode Framework, minimum required parking is utilized with shared parking and reductions for provisions for cyclists, with metrics representing average acceptable rates.

Minimum Required Parking – This is the most common method of parking regulation. The SmartCode Framework assumes that the market is the best judge of the parking required to sell houses and lease commercial space. The minimum requirements seek to not over-encumber developers with the cost of providing more parking than necessary.

Maximum Provided Parking – Many intensely urban areas use maximum parking restrictions where a many transportation options and mix of residential and non-residential uses results in below-average parking demand. Historic cities and neighborhoods also utilize this technique to ensure the historic fabric is not replaced with parking lots.

- e. Required automobile parking may be fulfilled in the following locations:
 - i. Within the same lot;
 - ii. On-street parking spaces located along lot lines;
 - iii. Within an adjacent shared parking lot; and
 - iv. In T5 and T6, within a shared parking facility located within 600 feet of the use.

5.5.3 Maximum Automobile Parking Provided

- a. Off-street automobile parking spaces may not exceed the following maximums, calculated as a percentage of the minimum required automobile parking spaces:
 - i. T3: n/a;
 - ii. T4: 140%; and
 - iii. T5 and T6: 140% for buildings up to 60,000 sq. ft. and 125% for buildings greater than 60,000 sq. ft.

TABLE 5F. BICYCLE PARKING REQUIREMENTS

	Minimum Spaces	Enclosed Spaces	On-street Spaces
Commercial			
Food and beverage	0.5 spaces / 1,000 sq.ft.	n/a	100% max.
Office	0.2 spaces / 1,000 sq.ft.	50% min. where more than 12 spaces are required	100% max.
Other Commercial	0.25 spaces / 1,000 sq.ft.	n/a	100% max.
Institutional			
All Assembly	0.2 spaces / 1,000 sq.ft.	0% min.	100% max.
Lodging			
All Lodging	1 space / 10 rooms	n/a	n/a
Residential			
8-20 units per lot	0.75 spaces / unit	25% min.	0% max.
Over 20 units per lot	0.5 spaces / unit	50% min.	0% max.
Other Uses			
All Other	No minimum requirement		

TABLE 5H. AUTOMOBILE PARKING REQUIREMENTS

	T2	T3	T4	T5	T6
Agricultural	No minimum				
Civic Space	No minimum				
Commercial	4 / 1,000 sq.ft.		4 / 1,000 sq.ft.	3 / 1,000 sq.ft.	
Entertainment	Not applicable		Not applicable	1 / 6 fixed seats or patrons	
Industrial & Manufacturing	Not applicable		Not applicable	1 / 4 employees	
Institutional	1 / 4 fixed seats or patrons			1 / 8 fixed seats or patrons	
Lodging	1 / room		1 / room	0.5 / room	
Residential					
Home occupations	2 / 1,000 sq.ft.			1 / 1,000 sq.ft.	
Acessory dwellings	1 / ADU		No minimum		
1 & 2 dwelling units per lot	2 / unit		1.5 / unit		
over 2 dwelling units per lot	Not applicable		1.5 / unit	1 / unit	

TABLE 5I. PARKING OCCUPANCY RATE TABLE

	Weekdays			Weekends		
	8am-5pm	6pm-12am	12am-6am	8am-5pm	6pm-12am	12am-6am
Commercial	90%	80%	5%	100%	70%	5%
Food & Beverage	70%	100%	10%	70%	100%	20%
Entertainment	40%	80%	10%	80%	100%	10%
Industrial & Manufacturing	100%	20%	5%	5%	5%	5%
Institutional	100%	20%	5%	10%	10%	5%
Convention	100%	100%	5%	100%	100%	5%
Performing Arts	40%	80%	10%	80%	100%	10%
Religious	10%	5%	5%	100%	50%	5%
Lodging	70%	100%	100%	70%	100%	100%
Residential	60%	100%	100%	80%	100%	100%

5.5.4 Off-street Automobile Parking Design

- a. All off-street parking spaces and aisles must meet AASHTO size and configuration standards.
- b. Automobile parking must be located according to [Table 5L – Table 5P](#).
- c. Off-street parking must be accessed by rear alleys or rear lanes where available.
- d. Where rear alleys or rear lanes are not available, off-street parking may be accessed from the following locations:
 - i. From secondary frontages driveways should be located near the rear lot line; and
 - ii. Where secondary frontages are not available, parking may be accessed from the primary frontage.
- e. Driveways providing access to off-street parking are limited to **10** feet in width in T3 and T4, and **24** feet in T5 and T6.
- f. Pedestrian access to off-street parking must be provided from sidewalks along front or side street lot line.
- g. Off-street parking facilities must have a minimum vertical clearance of **7** feet. Where such a facility is to be used by trucks or for loading, the minimum ground floor clearance is **15** feet.
- h. Parking lots and structures must be screened along front and side street lot lines by one or more of the following methods:
 - i. Liner buildings, optional at parking lots and required at parking structures. A minimum of **70%** of parking structure width must have a screened ground floor frontage;
 - ii. A masonry wall no less than **4** feet in height; and
 - iii. A metal fence with a hedge or other landscape element to screen the view of parking.

Municipality

5.6 Landscaping Standards

- 5.6.1 The spacing and placement of plants must be adequate for the typical size, shape, and habit of the plant species at maturity.
- 5.6.2 Trees must meet the following clearances:
- 2 feet from walkways, curbing, and other impervious pavements when planted in a tree well or continuous planter;
 - 3 feet from walkways, curbing, and other impervious pavements when planted in a continuous swale;
 - 5 feet from street lights, underground utilities, utility meters and service lines, fences, walls, and other ground level obstructions;
 - 6 feet from porch eaves, awnings, and similar overhead obstructions associated with the ground level of buildings;
 - 8 feet from balconies, verandas, building eaves and cornices, and similar overhead obstructions associated with the upper stories of buildings;
 - 8 feet of vertical clearance above sidewalks; and
 - 13 feet of vertical clearance above roads and parking lots.
- 5.6.3 In T3 and T4, trees must be planted within the front setback as follows:
- In T3, 2 understory trees must be planted, min;
 - In T4 1 understory tree must be planted, min.; and
 - 10 shrubs may be substituted for 1 understory tree.
- 5.6.4 Preservation of existing trees and vegetation is encouraged and may be used to fulfill the landscape requirements.
- 5.6.5 Planting with spines, thorns, or needles that may present hazards are prohibited within 2 feet of front and sidestreet lot lines.
- 5.6.6 Artificial plants or artificial turf are prohibited, excluding active recreation sports fields that are typically subject to intense use and soil compaction which prohibits the establishment of turfgrass.
- 5.6.7 Noxious or invasive plants species must be removed.
- 5.6.8 Porous paving materials should be used in order to increase storm water infiltration on site.

Landscape

This section is not a complete landscape ordinance with installation and maintenance regulations. That may be added here if desired.

5.7 Fencing

- 5.7.1 When erected on a lot line, all of the fence and any of its supporting structures must be contained within the lot.
- 5.7.2 Barbed wire, razor wire, and electrically charged fences are not permitted.
- 5.7.3 Fences may not exceed 6 feet in height.

5.8 Signage Standards

- 5.8.1 This section establishes the standards for the number, size, placement, and physical characteristics of on-premise signs visible from a public sidewalk or adjacent property.
- 5.8.2 These regulations do not restrict the content of signs.
- 5.8.3 Permits required.
 - a. All signs with the exception of the following require a sign permit:
 - i. A-frame ground sign;
 - ii. Small signs. Any sign that does not exceed four square feet in area;
 - iii. Official sign. Notices required by law, signs erected by a governmental agency or public utility; and
 - iv. Temporary signs. Not more than one temporary sign is permitted per frontage and may not exceed 32 square feet in area. Temporary signs must be removed within 48 hours after the event. Temporary signs may not be displayed longer than the event or 30 days.
- 5.8.4 Nonconforming signs.
 - a. If a sign display is not current, is in a state of disrepair or is left blank for a continuous period of 30 days, it is considered abandoned, and within 30 days after abandonment the owner of the property must remove or replace the sign or the message.
 - b. Illumination may not be added to a nonconforming sign.
 - c. A nonconforming sign may not be replaced except to bring it into complete conformance with this section.
 - d. If a sign is removed or damaged to the extent that the cost of restoration exceeds 50 percent of the original value of the sign it must be removed or brought into complete conformance with this section.
- 5.8.5 Prohibited signs.
 - a. Signs with the following features are prohibited:
 - i. Off-premise signs;

Signage

This section has been simplified to comply with Reed vs. the Town of Gilbert, AZ. It regulates size, location, and type of sign, but does not regulate content.

Municipality

- ii. Signs utilizing animation or which contain the optical illusion of sign movement;
- iii. Signs incorporating noisy mechanical devices or emitting smoke or steam.
- iv. Inflatable signs, such as but not limited to balloons, gas inflated signs or similar signs;
- v. Portable signs, except a-frame ground signs; and
- vi. Any unsafe sign.

5.8.6 Permitted signs and restrictions.

- a. Permitted sign types are limited by district and the following restrictions according to [Table 5K. Sign Standards](#):
 - i. The number of signs per type;
 - ii. The area of signs; and
 - iii. The height of sign copy.
- b. All signs must meet the following clearance:
 - i. Minimum eight feet clear over sidewalks; and
 - ii. Minimum 10 feet clear over vehicular areas.
- c. All signs may be double-sided by right, if the configuration of the sign permits. Only the larger side contributes to the calculation of sign area.
- d. Illuminated signs are permitted as follows:
 - i. All signs may be illuminated by an external light source or internally illuminated if neon.

TABLE 5J. SIGN STANDARDS

Sign	Signs (max)	Area (max)	Copy Height (max)	T1	T2	T3	T4	T5	T6	SD
A-Frame	1 per tenant	9 sq.ft.	n/a		■			■	■	■
Awning	1 sloping plane & 1 valence per awning	75% awning area	16 in. on sloping plane, 8 in. on valence					■	■	■
Banner	1 per event	32 sq.ft.	n/a		■			■	■	■
Canopy	1 per canopy	2 sq.ft. per linear ft.	30 in.					■	■	■
Corner	1 per building	40 sq.ft.	n/a		■			■	■	■
Ground	1 per frontage	36 sq.ft.	n/a							■
Hanging	1 per tenant	6 sq.ft.	n/a					■	■	■
Marquee	1 per entry	4 sq.ft. per linear ft.	30 in.					■	■	■
Pole	1 per property	200 sq.ft.	30 in.							■
Mural	1 per frontage	no max.	n/a					■	■	■
Projecting	1 per tenant	6 sq.ft.	8 in.					■	■	■
Wall	1 per tenant	3 sq.ft. per linear ft.	18 in.					■	■	■
Window	1 per window	25% of glazed area	12 in.				■	■	■	■

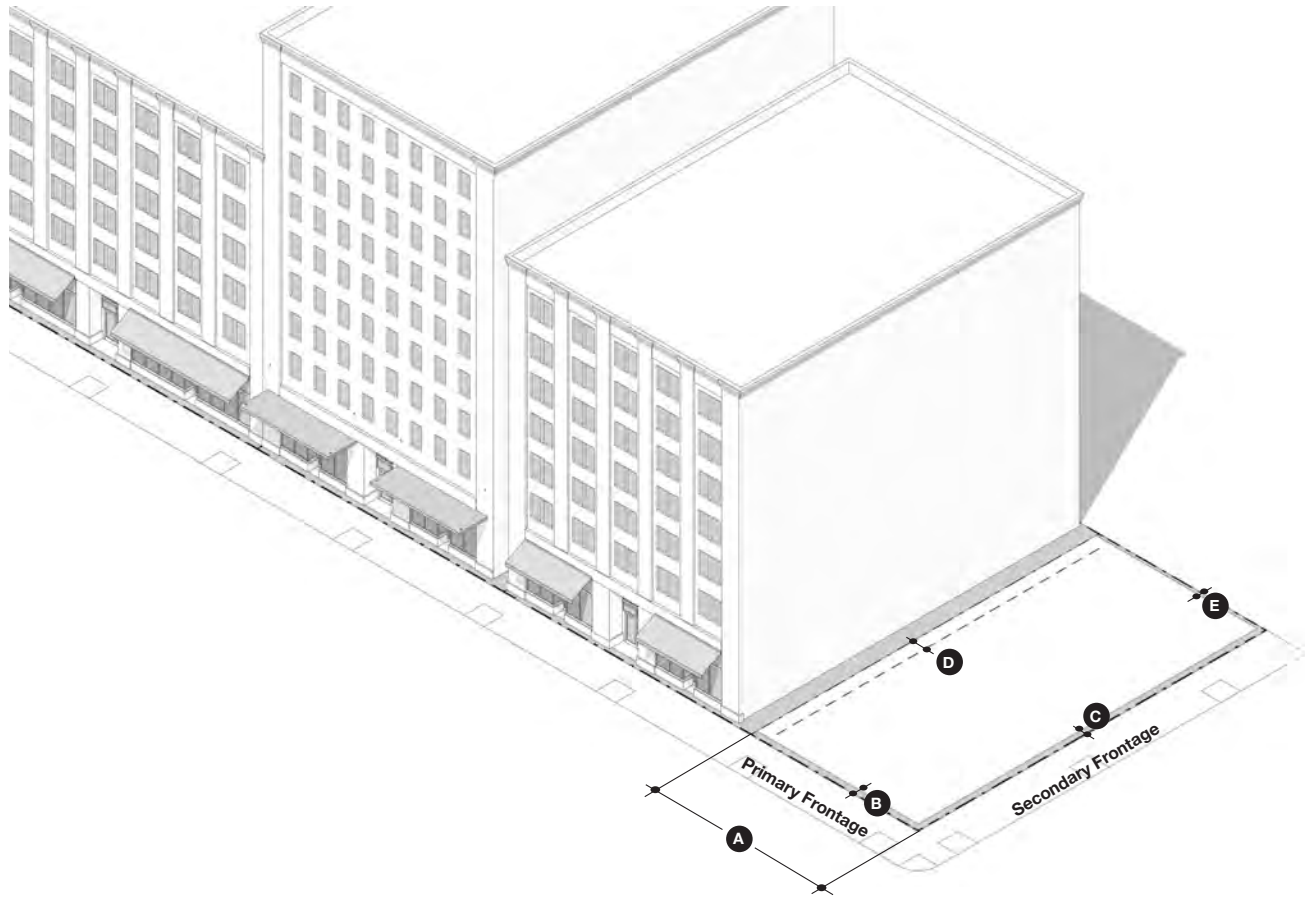
5.9 Special Conditions

- 5.9.1 The requirements of this section apply as indicated on the subdivision plat.
- 5.9.2 Frontage Type Requirement:
- a. A mandatory or recommended frontage type designation requires or advises that buildings provide a specific frontage type along front or side street lot lines.
- 5.9.3 Terminated Vistas:
- a. A mandatory or recommended terminated vista designation requires or advises that the building be provided with articulation of a type and character that responds visually to its axial location.
- 5.9.4 Cross-block Passages:
- a. A 10 foot or wider publicly accessible path must be provided through the lot.
- 5.9.5 Build-to Lines:
- a. A build-to line modifies the front or side street setback, requiring the facade of a building align with an indicated line.
- 5.9.6 B-Street Exemptions:
- a. Along designated B-Street, the following exemptions apply:
 - i. Parking is not required to be screened.
 - ii. Parking may be located between buildings and lot lines.
 - iii. In T5 and T6, buildings may occupy less than 70% of the lot width within the front setback.

Special Conditions

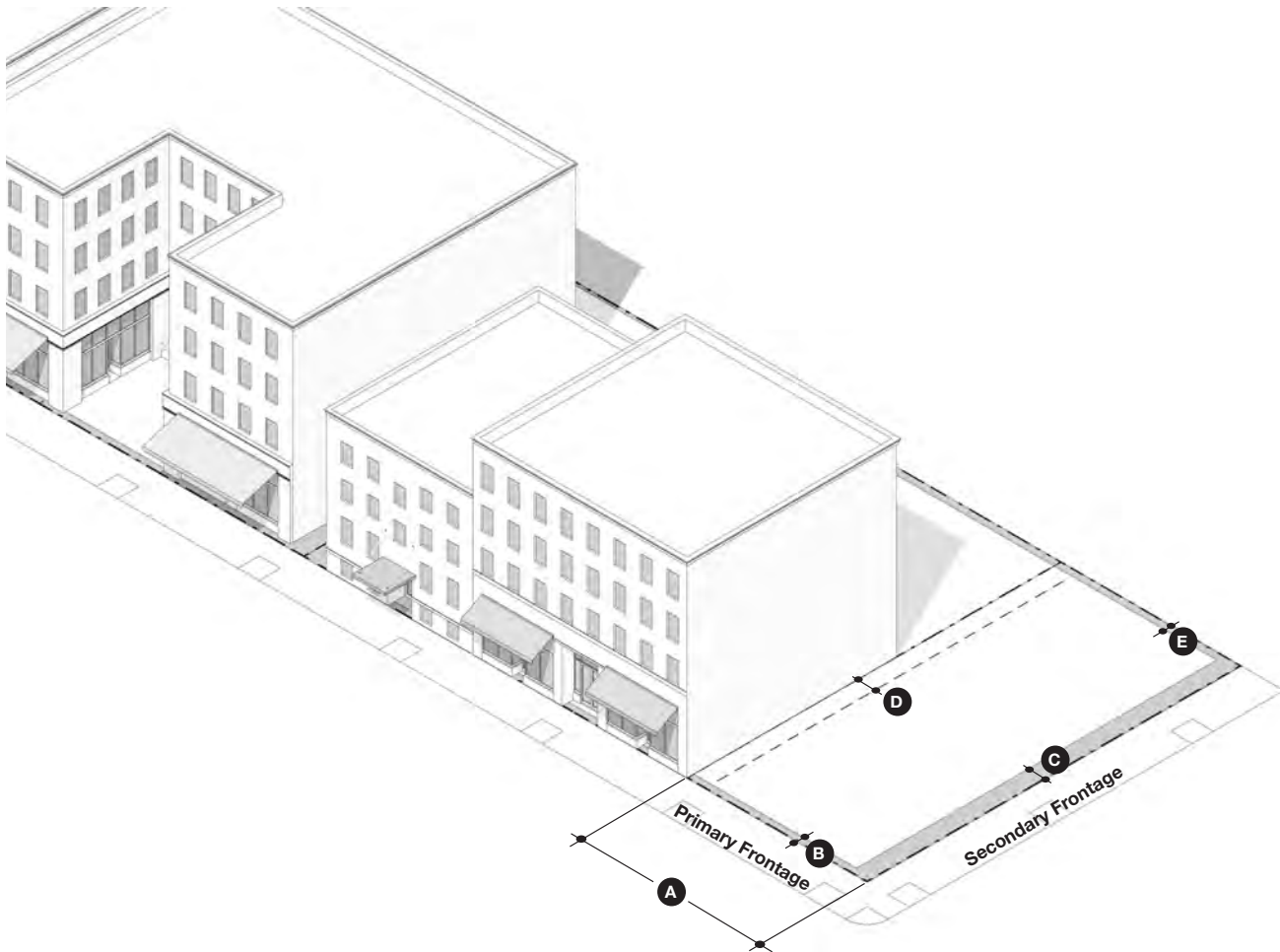
These conditions are commonly used by developers in large master plans, or in historic districts. They are more unusual in other contexts.

TABLE 5K. T6 STANDARDS



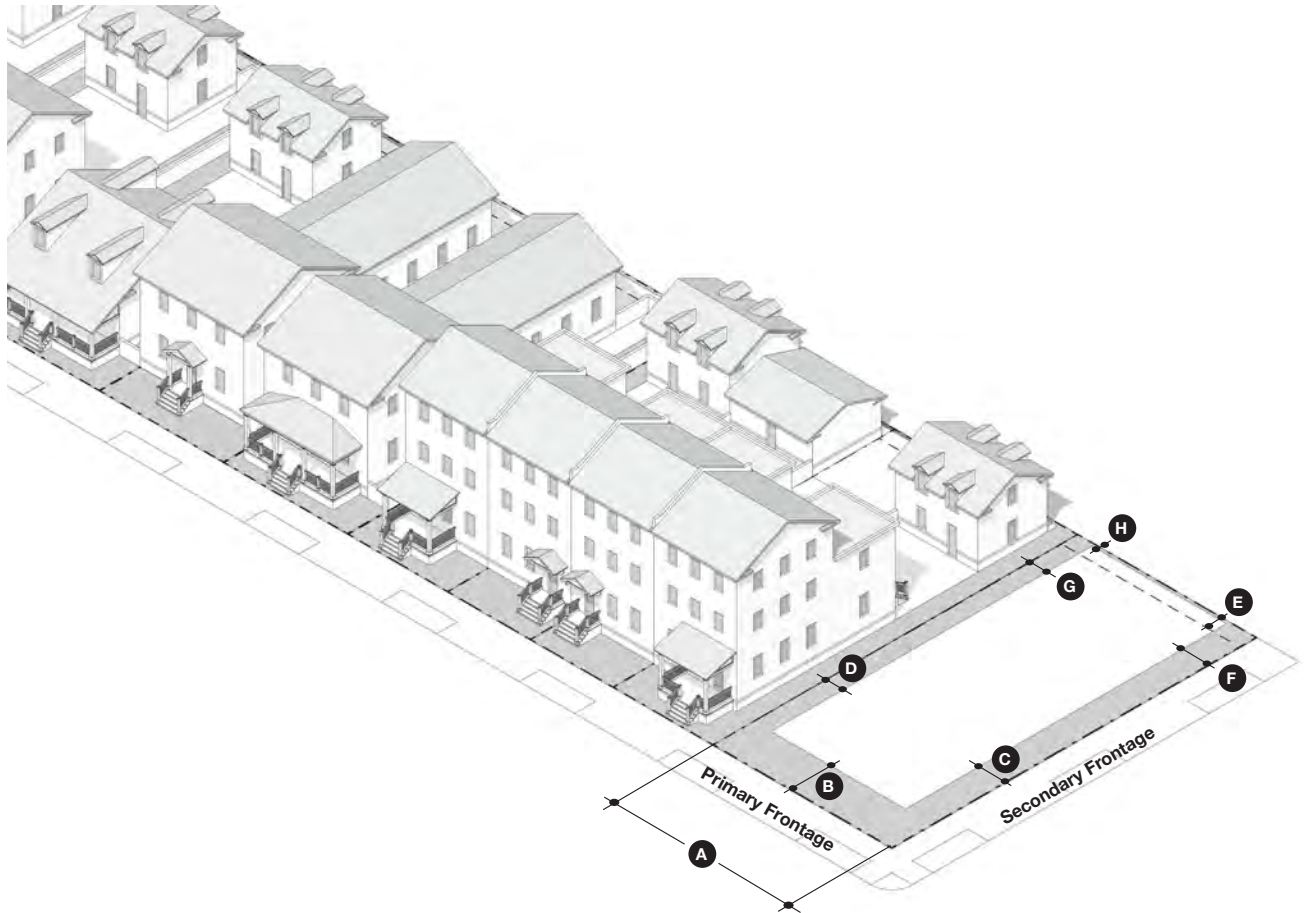
Lot Occupation		Accessory Structures	
A Lot Width	See §3.5.4	Front Setback	n/a
Lot Coverage	90% max.	Sidestreet Setback	n/a
Buildings		Side Setback	n/a
B Front Setback	2 ft. – 15 ft.	Rear Setback	n/a
C Sidestreet Setback	2 ft. – 15 ft.	Rear Alley Setback	n/a
D Side Setback	0 ft. or 5 ft. min.	Height	n/a
E Rear Setback	5 ft. min.	Parking	
E Rear Alley Setback	15 ft. min. from centerline	Front Setback	B + 20 ft. min., or B within liner buildings
Height	8 stories max.	Sidestreet Setback	8 ft. min.
Ground Floor Glazing	60% min.	Structure Height	50 ft. max.
Upper Floor Glazing	25% min.		
Entry Frequency	75 ft. max.		

TABLE 5L. T5 STANDARDS



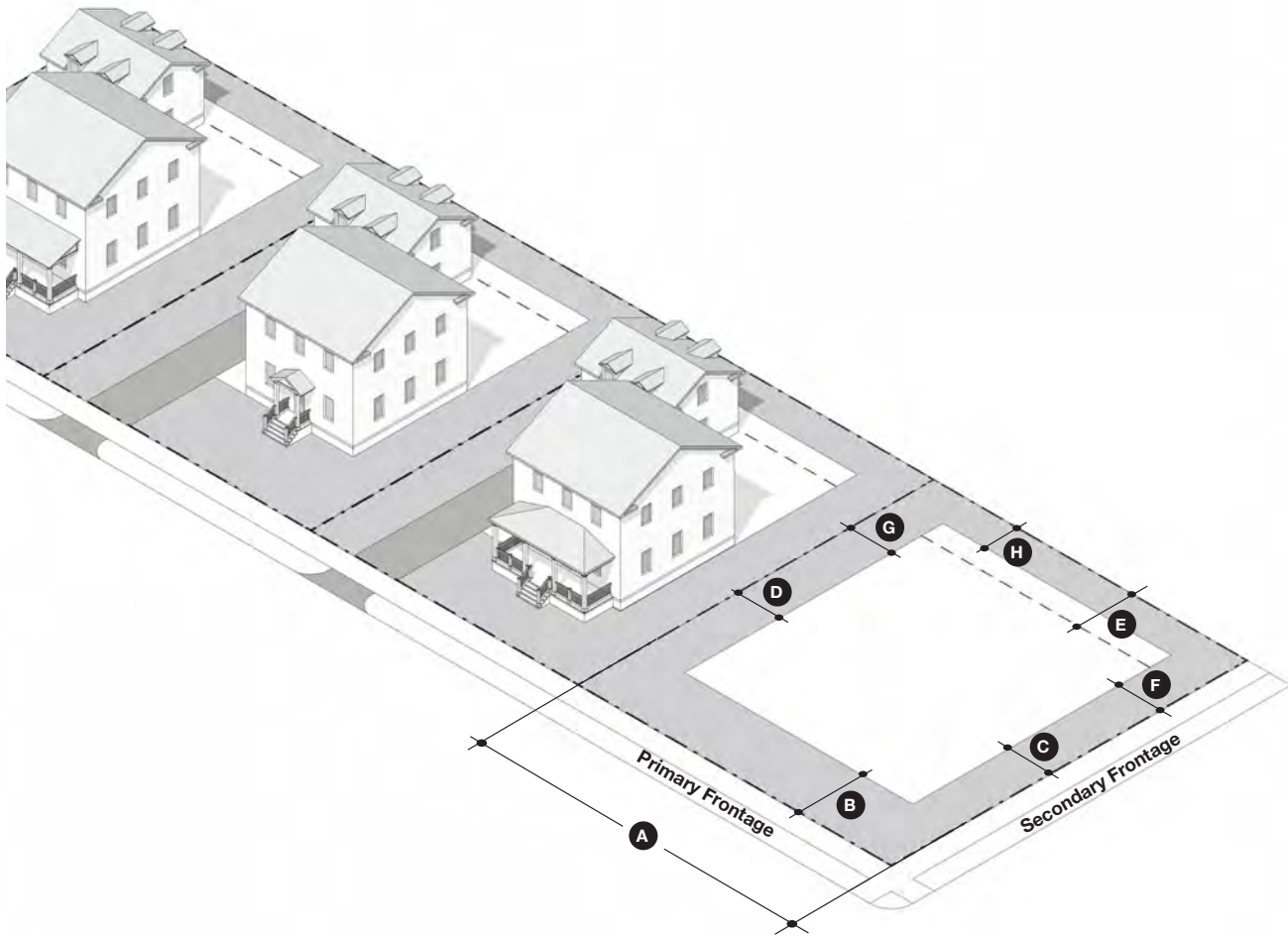
Lot Occupation		Accessory Structures	
A Lot Width	See §3.5.4	Front Setback	B + 20 ft. min.
Lot Coverage	90% max.	C Sidestreet Setback	2 ft. min.
Buildings		D Side Setback	0 ft. or 5 ft. min.
B Front Setback	2 ft. – 15 ft.	E Rear Setback	5 ft. min.
C Sidestreet Setback	2 ft. – 15 ft.	E Rear Alley Setback	15 ft. min. from centerline
D Side Setback	0 ft. or 5 ft. min.	Height	3 stories max.
E Rear Setback	5 ft. min.	Parking	
E Rear Alley Setback	15 ft. min. from centerline	Front Setback	B + 20 ft. min., or B within liner buildings
Height	5 stories max.	Sidestreet Setback	8 ft. min.
Ground Floor Glazing	70% min.	Structure Height	50 ft. max.
Upper Floor Glazing	25% min.		
Entry Frequency	75 ft. max.		

TABLE 5M. T4 STANDARDS



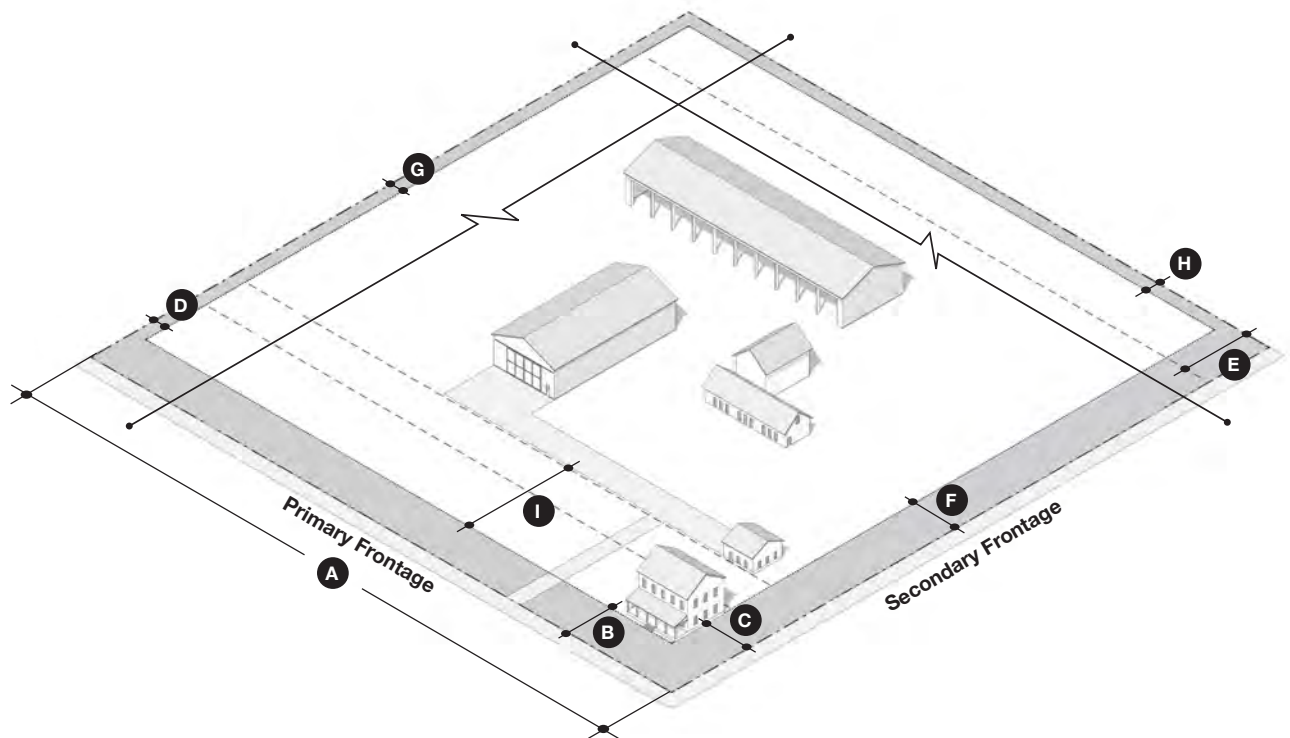
Lot Occupation		Accessory Structures	
A Lot Width	See §3.5.4	Front Setback	B + 20 ft. min.
Lot Coverage	70% max.	F Sidestreet Setback	2 ft. min.
Buildings		G Side Setback	0 ft. or 5 ft. min.
B Front Setback	6 ft. - 18 ft.	H Rear Setback	5 ft. min.
C Sidestreet Setback	6 ft. - 18 ft.	H Rear Alley Setback	15 ft. min. from centerline
D Side Setback	0 ft. or 5 ft. min.	Height	2 stories max.
E Rear Setback	12 ft. min.	Parking	
E Rear Alley Setback	15 ft. min. from centerline	Front Setback	B + 20 ft. min., or B within liner buildings and avg. lot slope > 5%
Height	3 stories max.	Sidestreet Setback	6 ft. min.
Ground Floor Glazing	15% min.	Structure Height	n/a
Upper Floor Glazing	15% min.		
Entry Frequency	n/a		

TABLE 5N. T3 STANDARDS



Lot Occupation		Accessory Structures	
A Lot Width	See §3.5.4	Front Setback	B + 20 ft. min.
Lot Coverage	60% max.	F Sidestreet Setback	6 ft. min.
Buildings		G Side Setback	6 ft. min.
B Front Setback	20 ft. min.	H Rear Setback	6 ft. min.
C Sidestreet Setback	12 ft. min.	H Rear Alley Setback	15 ft. min. from centerline
D Side Setback	8 ft. min.	Height	2 stories max.
E Rear Setback	12 ft. min.	Parking	
E Rear Alley Setback	15 ft. min. from centerline	Front Setback	B + 20 ft. min.
Height	2 stories max.	Sidestreet Setback	6 ft. min.
Ground Floor Glazing	n/a	Structure Height	n/a
Upper Floor Glazing	n/a		
Entry Frequency	n/a		

TABLE 50. T2 STANDARDS



Lot Occupation		Accessory Structures	
A	Lot Width	See §3.5.4	
	Lot Coverage	10% max.	
Buildings			
B	Front Setback	30 ft. min.	
C	Sidestreet Setback	30 ft. min.	
D	Side Setback	50 ft. min.	
E	Rear Setback	50 ft. min.	
	Rear Alley Setback	n/a	
	Height – residential	2 stories max.	
	Ground Floor Glazing	n/a	
	Upper Floor Glazing	n/a	
	Entry Frequency	n/a	
I	Front Setback	B + 50 ft. min.	
F	Sidestreet Setback	24 ft. min.	
G	Side Setback	20 ft. min.	
H	Rear Setback	10 ft. min.	
	Rear Alley Setback	n/a	
	Height	n/a	
		Parking	
		Front Setback	B + 20 ft. min.
		Sidestreet Setback	n/a
		Structure Height	n/a

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Municipality

ELEMENT 6 RIGHT-OF-WAY REGULATIONS**6.1 General**

- 6.1.1 Right-of-way designs submitted for approval must demonstrate compliance with this [Element](#) and the following:
- The Manual on Uniform Traffic Control Devices (MUTCD); and
 - [Municipality](#) road construction specifications.
- 6.1.2 Rights-of-way must be available for public use at all times; gates and other means of restricting access are not permitted, except by temporary street closure permit.
- 6.1.3 Additional licenses, agreements, and permits may be required as maintained and administered in the office of the respective [municipal](#) department.
- 6.1.4 Right-of-way that intersect with state owned roadways require coordination with the [State](#) Department of Transportation.

6.2 Context

- 6.2.1 The design standards for transportation facilities differ by area type.
- 6.2.2 Area type is determined by the transect district a Right-of-way passes through, as specified in [Table 6A. Area Types](#).
- 6.2.3 Roads are applicable within rural contexts.
- 6.2.4 Streets are applicable within walkable urban contexts.
- 6.2.5 Suburban contexts may utilize road standards in low density areas and street standards in moderate, high density, and commercial areas.
- 6.2.6 Right-of-way users, uses, and related geometries are prioritized by context as specified in [Table 6B. Use prioritization](#).

TABLE 6A. AREA TYPES

	T1	T2	T3	T4	T5	T6	SD
Rural	■	■					■
Suburban							■
Walkable Urban			■	■	■	■	■

■ Permitted

TABLE 6B. USE PRIORITIZATION

	T1	T2	T3	T4	T5	T6	SD
Pedestrian	4	4	2	1	1	1	?
Cyclist	3	3	3	2	2	3	?
Transit	6	6	5	4	3	2	?
Automobile	2	2	4	5	4	4	?
Truck	1	1	6	6	6	5	?
Green Infrastructure	5	5	1	3	5	6	?

Use Prioritization

Use prioritization changes through Transect district and establishes the purpose of the right-of-way. This assists in assembly selection. SD must be developed for the local context and purpose of the Special District it serves.

6.3 Right-of-way Assemblies

- 6.3.1 Rights-of-way are configured as an assembly of elements consisting of:
- A roadway, and
 - Public frontages to either side of the roadway.
- 6.3.2 Assemblies are limited by type, according to [Table 6C. Assembly Types](#).
- 6.3.3 Design of the assembly must change along with the underlying transect district.
- 6.3.4 Where transect districts to either side of the assembly differ:
- The design of the public frontage must match the standards of the district it is within or adjacent to; and
 - The design of the roadway must match the standards of the most intense it is within or adjacent to.
- 6.3.5 Rights-of-way must be provided of sufficient width to accommodate all elements of the roadway and public frontages.
- 6.3.6 Where redesign of existing facilities results in excess right-of-way, the additional width should be given to the public frontages.

TABLE 6C. ASSEMBLY TYPES

		T1	T2	T3	T4	T5	T6	SD
Roads	Highway	■	■					■
	Rural Road	■	■	■				■
Streets	Neighborhood Street			■	■	■		■
	Commercial Street					■	■	■
	Drive			■	■	■	■	■
	Avenue			■	■	■	■	■
	Boulevard			■	■	■	■	■

■ Permitted

6.4 Roadway Requirements

6.4.1 Design Speed

- a. Roadways must be designed in accordance with the design speeds specified in [Table 6D. Design Speed](#).
- b. Posted speed should match design speed.

6.4.2 Automobile Movement

- a. Automobile movement is limited according to [Table 6E. Design Movement](#).
- b. 1-way movement is limited to:
 - i. Paired assemblies to either side of civic space or natural area; and
 - ii. Modifications of existing facilities where the right-of-way is constrained.

6.4.3 Automobile Travel Lanes

- a. The number of automobile travel lanes is limited according to [Table 6F. Travel Lane Count](#).
- b. The width of automobile travel lanes is limited according to [Table 6G. Travel Lane Size](#) except as follows:
 - i. Along bus routes, outside curb or parking adjacent lanes must be a minimum of 11 feet;
 - ii. Travel lanes adjacent to 45-degree on-street parking must be a minimum of 12 feet in width; and
 - iii. Travel lanes adjacent to 60-degree on-street parking must be a minimum of 14 feet in width.

- c. Continuous left turn lanes are limited to the redesign of existing facilities.
- d. Automobile travel lanes are measured to the edge of pavement.

6.4.4 On-street Parking Lanes

- a. On-street parking lane type is limited according to [Table 6H. On-street Parking Type](#).
- b. Angled parking lanes must be 18 feet or less in width.
- c. Parallel parking lane width is limited according to [Table 6I. Parallel Parking Lanes](#).
- d. On-street parking lanes are measured to the face of curb, or edge of pavement where no curb exists.

6.4.5 Medians

- a. Medians are required within avenues.
- b. Medians are optional within highways and boulevards.
- c. Medians must be a minimum of 16 feet in width.

6.4.6 Multi-way Boulevards

- a. Boulevards may be adjusted to multi-way boulevards by adding medians and access lanes between the roadway and public frontages.
- b. Medians between the roadway and public frontages must be a minimum of 12 feet in width.
- c. The roadway must be configured as follows:
 - i. The design speed is limited to 45 mph max.;
 - ii. Automobile travel lanes are limited to 2 - 6, excluding access lanes;
 - iii. On-street parking is not permitted within the roadway; and
 - iv. Bicycle facilities are limited to Class 4.
- d. Access lanes must be configured as follows:
 - i. The design speed is limited to 15 - 25 mph;
 - ii. Automobile travel lanes are limited to 1;
 - iii. Parallel or angled parking is required along the outside edge of access lanes and optional along the inside edge; and
 - iv. Bicycle facilities may be Class 2, 3, or 4.

Municipality

6.4.7 Bicycle Facilities

- a. Continuity must be provided for bicycle facilities that terminate at site boundaries.
- b. Bicycle facilities must be configured by facility type, according to [Table 6J. Bicycle Facilities](#).

TABLE 6D. DESIGN SPEED

	T1	T2	T3	T4	T5	T6	SD
Highway	55 mph max.	55 mph max.					55 mph max.
Rural Road	45 mph max.	35 - 45 mph	30 mph max.				45 mph max.
Neighborhood Street			15 - 25 mph	15 - 25 mph	15 - 30 mph		30 mph max.
Commercial Street					20 - 35 mph	20 - 35 mph	35 mph max.
Drive			20 - 35 mph	20 - 35 mph	20 - 35 mph	20 - 35 mph	35 mph max.
Avenue			20 - 35 mph	20 - 35 mph	20 - 35 mph	20 - 35 mph	35 mph max.
Boulevard			30 - 45 mph	30 - 45 mph	30 - 45 mph	30 - 45 mph	45 mph max.

TABLE 6E. DESIGN MOVEMENT

	T1	T2	T3	T4	T5	T6	SD
2-way Yield			■	■	■		■
2-way Slow			■	■	■	■	■
1-way Slow			■	■	■	■	■
2-way Low	■	■			■	■	■
1-way Low	■				■	■	■
2-way High	■						■

■ Permitted

TABLE 6F. TRAVEL LANE COUNT

	T1	T2	T3	T4	T5	T6	SD
Highway	2 - 4	2 - 4					2 - 4
Rural Road	1 - 2	2	1 - 2				1 - 2
Neighborhood Street			1 - 2	1 - 2	1 - 2		1 - 2
Commercial Street					1 - 4	1 - 4	1 - 4
Drive			1 - 2	1 - 2	1 - 2	1 - 2	1 - 2
Avenue			2	2	2 - 4	2 - 4	2 - 4
Boulevard			4 - 6	4 - 6	4 - 6	4 - 6	4 - 6

TABLE 6G. TRAVEL LANE SIZE

	T1	T2	T3	T4	T5	T6	SD
Yield Movement			14 - 18 ft.	14 - 18 ft.	16 - 18 ft.		14 - 18 ft.
Slow Movement			8 - 10 ft.	8 - 10 ft.	9 - 11 ft.	9 - 11 ft.	8 - 11 ft.
Low Movement	11 - 13 ft.	10 - 12 ft.			10 - 12 ft.	10 - 12 ft.	10 - 13 ft.
High Movement	12 - 14 ft.						12 - 14 ft.

TABLE 6H. ON-STREET PARKING TYPE

	T1	T2	T3	T4	T5	T6	SD
Highway	n/a	n/a					n/a
Rural Road	n/a	shoulder	shoulder				shoulder
Neighborhood Street			parallel	parallel	parallel		parallel
Commercial Street					parallel or angled	parallel or angled	parallel or angled
Drive			parallel	parallel	parallel or angled	parallel or angled	parallel or angled
Avenue			parallel	parallel or angled	parallel or angled	parallel or angled	parallel or angled
Boulevard			parallel	parallel	parallel	parallel	parallel

TABLE 6I. PARALLEL PARKING LANES

	Lane Width
Yield Movement	7 ft.
Slow Movement	7 - 8 ft.
Low Movement	8 ft.
High Movement	n/a

TABLE 6J. BICYCLE FACILITIES

	Facility Width	Buffer
Shared Lane	n/a	n/a
Dedicated Lane	5 ft. min.	n/a
Buffered Lane	5 ft. min.	3 ft. min.
Protected Lane	6 ft. min.	3 ft. min.
Shared-use Trail	10 ft. min. 2-way	n/a

6.5 Public Frontage Requirements

- 6.5.1 Public frontages consist of sidewalks, planting and furnishing areas, and the roadway edge.
- 6.5.2 Sidewalks
- Sidewalks width must meet the standards specified in [Table 6K. Minimum Sidewalk Width](#).
 - At intersections, sidewalks must connect to 2 separate ramps per intersection quadrant.
- 6.5.3 Planting and Furnishing Area
- Planting and furnishing areas are required by type as specified in [Table 6L. Planting and Furnishing Area Types](#).
 - Planting and furnishing areas must meet the minimum width standards of [Table 6M. Minimum Planting and Furnishing Area Width](#) and the following:
 - A verge is required, providing allowable locations for infrastructure and furniture outside of pedestrian, bicycle, public transportation, and vehicular access ways;
 - The verge may overlap into the sidewalk, provided a minimum 5 foot wide pedestrian path is maintained;
 - Lighting must be located within the verge;

Terminology

Terms like public frontage will need to be replaced with the local term. The public frontage is a term historically used by the model SmartCode to describe the space between the right-of-way and the outside curb. This update has adopted this term for convenience.

- iv. In T5 and T6 where the right-of-way is 45 feet wide or less or commercial streets 60 feet wide or less the planting and furnishing area may exclude planting;
- v. Along commercial streets, the spacing of trees may be irregular to avoid visually obscuring shopfronts;
- vi. Tree spacing may be adjusted up to 20% by administrative waiver to accommodate specific site conditions; and
- vii. Bioswales are not permitted in T5 or T6.
- c. Bicycle racks must meet the following requirements:
 - i. Bicycle racks must be configured to provide two points of contact for locking bicycles.
 - ii. Bicycle racks may not be located within the following areas:
 - (a) Within 5 feet of fire hydrants;
 - (b) Within 4 feet of loading zones and bus stop markers;
 - (c) Within 3 feet of driveways and manholes; and
 - (d) Within 2 feet of utility meters and tree boxes.
 - iii. Bicycle rack placement may not reduce the pedestrian sidewalk path to less than 5 feet accounting for a 56cm bicycle properly locked to the rack.
 - iv. Bicycle racks installed parallel to the curb must be set back from the curb a minimum of 3 feet.
 - v. Bicycle racks installed perpendicular to the curb must allow for a minimum setback of 2 feet between the curb and a 56cm bicycle properly locked to the rack.

6.5.4 Curbs

- a. Vertical curbing is required in T4, T5, and T6, and optional in T3 and SD.
- b. Roll-over curbing is optional in T3 and SD.
- c. Where automobile travel lanes are directly adjacent to curbs or where curb extensions are used, the minimum curb return radius is as specified in [Table 6N. Curb Return Radius\(a\)](#).
- d. Where automobile travel lanes are not directly adjacent to curbs, the minimum curb return radius is determined as follows:
 - i. The minimum curb return radius specified in [Table 6N. Curb Return Radius\(a\)](#) is reduced

Municipality

by accounting for the actual path of vehicle tracking;

- ii. The minimum curb return radius must not be reduced below the radii specified in [Table 6N. Curb Return Radius](#)(b) and (c);
- iii. Where on-street parking or bike lanes are on only one leg of an intersection quadrant, the minimum curb return radius is reduced according to the formula in [Table 6N. Curb Return Radius](#)(d);
- iv. Where on-street parking or bike lanes are on both legs of an intersection quadrant in a symmetric configuration, the minimum curb return radius is reduced according to the formula in [Table 6N. Curb Return Radius](#)(e); and
- v. Where on-street parking or bike lanes are on both legs of an intersection quadrant in an asymmetric configuration, the minimum curb return radius is reduced according to the formula in [Table 6N. Curb Return Radius](#)(f).

TABLE 6K. MINIMUM SIDEWALK WIDTH

	T1	T2	T3	T4	T5	T6	SD
Highway	n/a	n/a					n/a
Rural Road	n/a	n/a	5 ft.				n/a
Neighborhood Street			5 ft.	6 ft.	6 ft.		5 ft.
Commercial Street					10 ft.	14 ft.	10 ft.
Drive			6 ft.	8 ft.	8 ft.	10 ft.	6 ft.
Avenue			6 ft.	8 ft.	8 ft.	10 ft.	6 ft.
Boulevard			6 ft.	8 ft.	10 ft.	14 ft.	6 ft.

TABLE 6L. PLANTING AND FURNISHING AREA TYPES

	T1	T2	T3	T4	T5	T6	SD
Highway	Swale	Swale					Swale
Rural Road	Swale	Swale	Swale or Continuous				Swale or Continuous
Neighborhood Street			Swale or Continuous	Continuous or Long Planter	Long Planter or Urban	Long Planter or Urban	Any
Commercial Street					Urban or Arcaded	Urban or Arcaded	Urban or Arcaded
Drive Avenue Boulevard			Continuous or Long Planter	Continuous or Long Planter	Long Planter, Urban, or Arcaded	Long Planter, Urban, or Arcaded	Any except swale

TABLE 6M. MINIMUM PLANTING AND FURNISHING AREA WIDTH

	T1	T2	T3	T4	T5	T6	SD
Swale	15 ft.	.15 ft.	10 ft.				10 ft.
Planter			8 ft.	8 ft.			6 ft.
Long Planter			8 ft.	6 ft.	6 ft.	8 ft.	6 ft.
Urban					5 ft.	6 ft.	5 ft.
Arcaded					Integrated within the Arcade		

TABLE 6N. CURB RETURN RADIUS

	(a) Min. Effective Radius	Min. Radius Limit		Radius Reduction Formulae		
		(b) Curbed	(c) No Curb	(d) 1 Leg	(e) 2 Legs, Symmetric	(f) 2 Legs, Asymmetric
Highway	50 ft.	15 ft.	20 ft.	reduced radius = minimum effective radius - (parking lane width + bike lane width)	reduced radius = minimum effective radius - 3 x (parking lane width + bike lane width)	reduced radius = minimum effective radius - 3 x (average width of combined parking and bike lanes on both legs)
Rural Road	50 ft.	15 ft.	20 ft.			
Neighborhood Street	25 ft.	5 ft.	15 ft.			
Commercial Street	35 ft.	5 ft.	15 ft.			
Drive	35 ft.	5 ft.	15 ft.			
Avenue	25 ft.	5 ft.	15 ft.			
Boulevard	50 ft.	10 ft.	20 ft.			

Municipality**6.6 Alleys**

- 6.6.1 Alleys are easements that provide access to properties from within a block.
- 6.6.2 Alleys are required to provide access to properties in T4, T5, and T6, except where a single property occupies a full block.
- 6.6.3 Alleys are required to provide access to properties less than 45 feet wide.
- 6.6.4 Alleys are optional in T3 and SD.
- 6.6.5 In T5 and T6, alleys must be configured as follows:
 - a. Alley aprons should be tapered;
 - b. Alley easements must be a minimum of 28ft. clear;
 - c. Alleys must be fully paved; and
 - d. Drainage should be provided through inverted crown or sheet flow.
- 6.6.6 In T3 and T4, alleys must be configured as follows:
 - a. Alley aprons should be curved;
 - b. Alley easements must be a minimum of 24ft. clear;
 - c. Alleys must be paved a width of 12 to 16 feet; and
 - d. Drainage should be provided through swales, inverted crown, or sheet flow.

TABLE 60. NS-50-26 T3



Assembly		Roadway	
Assembly Type	Neighborhood Street	Design Speed	15-20 mph
Right-of-way	50 ft. min.	Movement	Yield
Transect Zones	T3	Travel Lanes	1, 12 ft. Shared
Public Frontage		Parking Lanes	2, 7 ft. Parallel
Sidewalk	5 ft. min.	Median	None
Planting & Furnishing	7 ft. min., Continuous	Bicycle Facilities	Shared Lane
Curb	Vertical	Class	Local

TABLE 6P. NS-50-26 T4



Assembly		Roadway	
Assembly Type	Neighborhood Street	Design Speed	15-20 mph
Right-of-way	50 ft. min.	Movement	Yield
Transect Zones	T4	Travel Lanes	1, 12 ft. Shared
Public Frontage		Parking Lanes	2, 7 ft. Parallel
Sidewalk	6 ft. min.	Median	None
Planting & Furnishing	6 ft. min., Long Planter	Bicycle Facilities	Shared Lane
Curb	Vertical	Class	Local

TABLE 6Q. NS-70-34 T3



Assembly		Roadway	
Assembly Type	Neighborhood Street	Design Speed	15-25 mph
Right-of-way	70 ft. min.	Movement	2-way Slow
Transect Zones	T3	Travel Lanes	2, 10ft. each
Public Frontage		Parking Lanes	2, 7 ft. Parallel
Sidewalk	5 ft. min.	Median	None
Planting & Furnishing	13 ft. min. Continuous	Bicycle Facilities	Shared Lane
Curb	Vertical	Class	Sub-collector or Local

TABLE 6R. NS-70-34 T4



Assembly		Roadway	
Assembly Type	Neighborhood Street	Design Speed	15-25 mph
Right-of-way	70 ft. min.	Movement	2-way Slow
Transect Zones	T4	Travel Lanes	2, 10 ft. each
Public Frontage		Parking Lanes	2, 7 ft. Parallel
Sidewalk	6 ft. min.	Median	None
Planting & Furnishing	12 ft. min., Cont. or Long	Bicycle Facilities	Shared Lane
Curb	Vertical	Class	Sub-collector or Local

TABLE 6S. NS-70-42 T4



Assembly		Roadway	
Assembly Type	Neighborhood Street	Design Speed	15-20 mph
Right-of-way	70 ft. min.	Movement	2-way Slow
Transect Zones	T4	Travel Lanes	2, 9 ft. each
Public Frontage		Parking Lanes	2, 7 ft. Parallel
Sidewalk	6 ft. min.	Median	20 ft. min.
Planting & Furnishing	8 ft. min., Cont. or Long	Bicycle Facilities	2, 5 ft. min. Dedicated
Curb	Vertical	Class	Sub-collector or Local

TABLE 6T. CS-70-42 T5/T6



Assembly		Roadway	
Assembly Type	Commercial Street	Design Speed	15-20 mph
Right-of-way	70 ft. min.	Movement	2-way Slow
Transect Zones	T5, T6	Travel Lanes	2, 9 ft. each
Public Frontage		Parking Lanes	2, 7 ft. Parallel
Sidewalk	7 ft. min.	Median	None
Planting & Furnishing	7 ft. min., Urban	Bicycle Facilities	2, 5 ft. min. Dedicated
Curb	Vertical	Class	Sub-collector or Local

TABLE 6U. AV-80-40 T3



Assembly		Roadway	
Assembly Type	Avenue	Design Speed	25-30 mph
Right-of-way	80 ft. min.	Movement	2-way Slow
Transect Zones	T3	Travel Lanes	2, 12 ft. each
Public Frontage		Parking Lanes	2, 8 ft. Parallel
Sidewalk	5 ft. min.	Median	16 ft. min.
Planting & Furnishing	7 ft. min., Continuous	Bicycle Facilities	Shared Lane
Curb	Optional	Class	Collector or lower

TABLE 6V. AV-80-40 T4



Assembly		Roadway	
Assembly Type	Avenue	Design Speed	25-30 mph
Right-of-way	80 ft. min.	Movement	2-way Slow
Transect Zones	T4	Travel Lanes	2, 12 ft. each
Public Frontage		Parking Lanes	2, 8 ft. Parallel
Sidewalk	6 ft. min.	Median	16 ft. min.
Planting & Furnishing	6 ft. min., Cont. or Long	Bicycle Facilities	Shared Lane
Curb	Vertical	Class	Collector or lower

TABLE 6W. CT-80-44 T5/T6



Assembly		Roadway	
Assembly Type	Commercial Street	Design Speed	15-25 mph
Right-of-way	80 ft. min.	Movement	2-way Slow
Transect Zones	T5, T6	Travel Lanes	2, 12 ft. each
Public Frontage		Parking Lanes	2, 7 ft. Parallel
Sidewalk	12 ft. min.	Median	None
Planting & Furnishing	6 ft. min., Urban	Bicycle Facilities	Shared Lane
Curb	Vertical	Class	Collector or lower

TABLE 6X. BL-160-28 T5/T6



Assembly		Roadway	
Assembly Type	Boulevard	Design Speed	15-40 mph
Right-of-way	160 ft. min.	Movement	2-way Slow and Low
Transect Zones	T5, T6	Travel Lanes	2 @ 12 ft., 4 @ 10 ft.
Public Frontage		Parking Lanes	2, 8 ft. Parallel
Sidewalk	10 ft. min.	Median	3 @ 16 ft. min. each
Planting & Furnishing	6 ft. min., Urban	Bicycle Facilities	Shared Lane
Curb	Vertical	Class	Arterial or Collector

Municipality

ELEMENT 7 ADMINISTRATION AND PROCESS**7.1 Approval Authority**

- 7.1.1 This section outlines the bodies with review, approval, and appellate authority in administering this [chapter](#).
- 7.1.2 [Table 7A. Approval Authority](#) outlines the chain of decision making and appeal for actions enabled by this [chapter](#).
- 7.1.3 [Municipal legislature](#).
 - a. The [municipal legislature](#) is the zoning authority for the [municipality](#) and has authority to make decisions as shown in [Table 7A. Approval Authority](#) and [Table 7B. Procedures Summary Table](#).
 - b. The [municipal legislature](#) has delegated ministerial approval of final plats to the [planning commission](#) if the [CRC](#) determines all conditions of this chapter have been satisfied.
- 7.1.4 [Planning commission](#).
 - a. The [planning commission](#) is a board nominated by the [municipal legislature](#) and appointed by the [mayor](#).
 - b. The [planning commission](#) is responsible to make recommendations or decisions on applications as shown in [Table 7A. Approval Authority](#) and [Table 7B. Procedures Summary Table](#).
- 7.1.5 [Consolidated review committee](#).
 - a. [Municipality](#) hereby creates a [Consolidated Review Committee \(“CRC”\)](#) comprised of a member from each regulatory agency having jurisdiction over the permitting of a project to process applications administratively.
 - b. The CRC membership includes:
 - i. [Planning and Development Director \(chair\)](#);
 - ii. [County Engineer](#);
 - iii. [Parks and Recreation Director](#);
 - iv. [Road Department Superintendent](#);
 - v. [Fire Department Director or the Fire Marshall from the fire department with jurisdiction over the project area](#); and
 - vi. [Chief of Police](#).

Administration and Process

This entire structure varies from stated to state and between local jurisdictions. This model is a combination of the most common situations and must be carefully calibrated to the local legal structure.

The goal for the administration and process is to have clear and objective standards within the code that can largely be approved by professional staff. The process should be simple and easy to navigate.

7.1.6 Board of appeals.

- a. The board of appeals has the following powers and duties:
 - i. To hear, review and determine appeals made by the CRC regarding the enforcement of this chapter;
 - ii. To hear and decide conditional uses of the zoning ordinance; and
 - iii. To authorize, upon appeal in specific cases, a variance to the zoning ordinance.

TABLE 7A. APPROVAL AUTHORITY

Review Authority Role				
	CRC	Planning Commission	Municipal Legislature	Board of Appeals
a. Administrative				
Verification of Zoning Compliance	A			AP
Interpretation	A			AP
b. Application Review				
Conditional Use	R		AP	A
Rezoning	R	R	A	AP
Concept plan	R			
Preliminary plat	R	R	A	AP
Final plat	R	R	A	AP
Building Permit	A		AP	
d. Legislative				
Subdivision Regulation Amendments	R	A	AP	
Zoning Reclassification	R	R	A	AP
Zoning Text Amendment	R	R	A	AP

R – Review

A – Approval

AP – Appeal

7.2 Amendments

- 7.2.1 The municipal legislature may introduce amendments to this ordinance.
- 7.2.2 Any person may present a petition to the planning commission requesting amendments to this ordinance.
- 7.2.3 Proposed ordinance and map amendments must be consistent with the intent of the municipality's comprehensive plan and must not have a negative effect on surrounding properties or the municipality as a whole.
- 7.2.4 Map amendments determined to be an adjustment to this ordinance and that change area characteristics may require a comprehensive plan amendment.

Notifications and hearings

State and local requirements for legislative and quasi-judicial actions vary from place to place, so this section must be locally calibrated.

Municipality

7.3 Notification and hearings

7.3.1 Notification and hearings are required for both subdivision and zoning actions enabled by this Code, as specified in [Table 7B. Procedures Summary Table](#).

TABLE 7B. PROCEDURES SUMMARY TABLE							
Actions	Public Notice				Hearings		
	Published	Mailed	Posted sign	Electronic mail	Planning Commission	Municipal legislature	Board of appeals
Preliminary plat	■	■	■	■	■	■	
Final plat	■			■	■		
Variance	■	■	■	■	■		
Rezoning	■	■	■	■	■	■	
Conditional use	■	■	■	■			■
Zoning amendment	■	Where applicable	Where applicable	■	Quasi-judicial	■	
Map amendment	■	Where applicable	Where applicable	■	■		
Appeal	■	Where applicable	Where applicable	■		Legislative appeals	BA appeals

7.3.2 Public notice

- a. Notice that is published, mailed, posted by sign, or electronically mail is required as shown on [Table 7B. Procedures Summary Table](#) and must comply with the following:
 - i. [Calibrate locally](#)

7.3.3 Public hearings

- a. The decision-making body may take any one of the following actions:
 - i. [Calibrate locally](#)

7.4 Processes**7.4.1 Subdivision**

- a. Unless otherwise specifically provided for, the [CRC's](#) review and [planning director's](#) approval and plat recording is required before any lot is sold or before any building permit in a proposed subdivision will be granted.
- b. The applicant must secure plat approval through the following procedure:
 - i. Hold a pre-application conference with the [planning department](#);
 - ii. Submit a concept plan to the [planning department](#) for [CRC](#) review, prepared in accordance with [§7.4.3](#) and [§7.5.3](#);

- iii. Submit a preliminary plat to the [planning department](#), prepared in accordance with [§7.4.4](#) and [§7.5.4](#) for [CRC](#) review and administrative approval by the [planning director](#);
- iv. Submit construction plans to the [planning department](#), prepared in accordance with [§7.4.5](#) and [§7.5.5](#) for [engineering department](#) approval; and
- v. Submit the final subdivision plat, prepared in accordance with [§7.4.6](#) and [§7.5.6](#) for ministerial approval by the [planning director](#).

7.4.2 Minor subdivision

- a. A minor subdivision is available for any subdivision that will result in 6 lots and that does not include the construction and dedication of a public street.
- b. Minor subdivisions may be approved administratively by the [planning director](#) and do not require concept plans or preliminary plats.

7.4.3 Concept plan

- a. A concept plan is required for any subdivision that will result in 6 or more lots or fewer than 6 lots which include the construction and dedication of a public street.
- b. Concept plans must include the information specified in [§7.5.3](#).
- c. The applicant must submit the concept plan for staff review. The [planning department](#) will advise the applicant of potential conflicts with this [chapter](#) and make recommendations.

7.4.4 Preliminary plat

- a. A preliminary plat is required for all subdivisions except minor subdivisions.
- b. Preliminary plats must include the information specified in [§7.5.4](#).
- c. Applicants must file a preliminary plat application with the [planning director](#) for approval following review by the [CRC](#).
 - i. Approval of the preliminary plat constitutes authorization to prepare and present detailed construction plans for the proposed development.
 - ii. All preliminary plat approvals are conditioned upon the approval of construction plans.

Municipality

- d. Minor revisions. The [planning director](#) may approve minor revisions that do not require a variance including, but not limited to:
 - i. Adjustments to the location of lot lines, streets, or open space, provided they remain in compliance with the regulations of this chapter;
 - ii. Adjustments to transect districts that remain in compliance with [Table 2C. Community Unit Type Requirements](#);
 - iii. Changes that do not significantly alter the overall layout of the plan and its basic development concept; and
 - iv. Minor revisions to preliminary plats only subject that portion under revision to review.
- e. Major revisions. The [planning commission](#) may approve changes that involve major revisions, following [CRC](#) review, including, but not limited to:
 - i. Modification to the pattern of streets or street connections, internal and external to the project area, that decrease connectivity or significantly alter traffic patterns, except changes that result directly from discovery of topographical or environmental obstacles that could not reasonably have been known at the time of initial plat approval;
 - ii. Changes affecting 10% or more of the open space, including but not limited to its area, location, accessibility, or degree of contiguity;
 - iii. Reduction in dedications to the public;
 - iv. Changes to subdivision boundaries that exceed 5% of the area of initial approval;
 - v. Changes that increase the area of disturbance of slopes of 25% or greater or increase the area of manipulation of the floodplain; and
 - vi. Changes that alter the overall layout of the plan and its basic development concept.

7.4.5 Construction plans

- a. The preparation and review of construction plans must be coordinated with the [planning department](#) and [engineering department](#).
- b. Construction plans may be presented concurrent with, or at any time after, a preliminary plat has been approved.
- c. Construction plans must include the information specified in [§7.5.5](#).

- d. Construction plans must:
 - i. Include the entire subdivision, or the entire phase or phases for which final approval will be sought; and
 - ii. Be prepared and submitted by a licensed engineer or architect.
- e. Before final plats are submitted for approval:
 - i. Revisions required by the [engineering department](#) must be accomplished; and
 - ii. All multi-phase developments are required to submit phasing and construction traffic plans for the entire development.

7.4.6 Final Plat

- a. A final plat is required for all subdivisions.
- b. Final plats must include the information specified in [§7.5.6](#).
- c. Applicants must file a final plat with the [planning commission](#) for approval.
- d. Final plats must:
 - i. Include the entire subdivision, or phase, for which final approval is sought;
 - ii. Substantially conform to the approved preliminary plat;
 - iii. Include all relevant plat certificates, including an owner's certificate which includes the owner's dedication of all easements and rights-of-way, streets, alleys, walkways, parks, and other open space as noted on the plat;
 - iv. Construction plans must be substantially complete, as determined by the [engineering department](#), prior to the granting of final plat approval by the [planning commission](#);
 - v. As a result of unforeseen conditions associated with a particular site, changes may be required in approved final plats. No change can be implemented and no associated construction may be initiated until a revised final plat and construction plans have been submitted to the [planning director](#); and
 - vi. The [planning director](#) may approve changes that involve minor adjustments to the location of lot lines, easements, building lines, streets, open space, notes, or similar matters, provided that all changes remain in compliance with this [chapter](#). Otherwise, any other changes or any deviations require [planning commission](#) approval of the revised plat.

Municipality

7.4.7 Rezoning

- a. Applications for map or text amendments are submitted to the [planning commission](#).
- b. Zoning map and text amendments must follow the current procedure as set forth under [state](#) law.
- c. Rezoning applications must include the information specified in [§7.5.2](#).
- d. One or more parcels or community units may be included in one rezoning request.
- e. Each community unit must meet district percentage mix requirements individually.
- f. Once the [legislative body](#) approves a new community rezoning under this ordinance, the parcels will be designated TC (Transect Community) on the zoning map until approval of the preliminary plat by the [planning director](#).
- g. Once the [planning director](#) approves the preliminary plat, zoning districts specified on the plat will replace the TC designation on the official zoning map as part of the preliminary plat approval.
 - i. The replacement of specific transect zoning districts on the official zoning map is not considered a zoning amendment; as such it does not require further action of the [planning commission](#) or [legislative body](#).
 - ii. Transect district placement is considered a continuation of the original TC rezoning process that was approved by official action of the [planning commission](#) and the [legislative body](#).
 - iii. Provided that transect district replacement of TC complies with the district percentage mix requirements for each community unit, the districts may be relocated within the parcels administratively and on the official zoning map until final plat approval.
 - iv. Once the final plat is approved, districts may not be relocated and may only be amended through the rezoning process.
 - v. In the event that within an approved TC a final plat is not approved, the property will be re-designated to the prior zoning on the zoning map without further action of the [planning commission](#) or [legislative body](#).

7.5 Applications

7.5.1 General

- a. Applications must include drawings clearly indicating the information specified in [Table 7C. Application Requirements](#).
- b. If the applicant is not the owner, a letter from the owner, a signature on the application, or a power of attorney is required for the applicant to act as agent with full authority.
- c. Applications including community units must provide a data table specifying the following:
 - i. Total site area;
 - ii. Type and area of each community unit and special district;
 - iii. Transect district and civic space acreages, and percentages for each community unit;
 - iv. Civic space type for each civic space;
 - v. Block perimeter length for each block; and
 - vi. Thoroughfare type for new communities.

7.5.2 Rezoning

- a. The following additional information must be provided to supplement the requirements of [Table 7C. Application Requirements](#):
 - i. A statement or narrative specifying how the rezoning request is consistent with the [municipal comprehensive plan](#).

7.5.3 Concept plan

- a. The information required in [Table 7C. Application Requirements](#) is modified as follows:
 - i. Boundary lines do not require a survey.
 - ii. The type of each street must be specified.
- b. The following additional information must be provided to supplement the requirements of [Table 7C. Application Requirements](#):
 - i. Special requirements, if any;
 - ii. A statement specifying how pedestrian, bicycle, transit, and automobile ways coordinate with adjoining development;
 - iii. A statement demonstrating availability of utilities, including water and sanitary sewer service (guarantee not required); and
 - iv. A statement demonstrating the presence of rare, threatened or endangered species, and

Municipality

the applicant's contact with the department of natural resources.

7.5.4 Preliminary plat

- a. Preliminary plat applications must be prepared and certified by a licensed land surveyor licensed to practice in the [state](#).
- b. The information required in [Table 7C. Application Requirements](#) is modified as follows:
 - i. The type of each street must be specified along with the street name.
 - ii. The drainage plan must indicate the following:
 - (a) Approximate volumes, direction of flows, and location of proposed detention and retention areas; and
 - (b) Culverts and associated drainage structure locations, sizes, and easements.
- c. The following additional information must be provided to supplement the requirements of [Table 7C. Application Requirements](#):
 - i. A statement specifying the owner and entity responsible for care, operation, and maintenance of all streets and civic spaces; and
 - ii. A statement specifying how pedestrian, bicycle, transit, and automobile ways coordinate with bordering land development.

7.5.5 Construction Plans

- a. Construction plan applications must be certified by a professional engineer or architect licensed to practice in the [state](#).
- b. The information required in [Table 7C. Application Requirements](#) is modified as follows:
 - i. The date and substance of any revisions to the plat must be indicated in a revision block drawn on the plat; and
 - ii. Utility plans must specify proposed water supply, fire protection, sanitary waste, reclaimed water, storm drainage, stormwater management facilities, gas lines, electric and telecom lines, poles, and equipment, and streetlights.
- c. The following additional information must be provided to supplement the requirements of [Table 7C. Application Requirements](#):
 - i. The required data table must include the following additional items:
 - (a) Total number of lots in each district with acreages and percentages;

- (b) Number of units per lot and total units; and
 - (c) Estimated lot coverage by district.
- ii. A statement indicating the approximate location, dimensions, and area of all property reserved for public use or for use of all property owners in the subdivision, and conditions for such conveyance or reservation;
- iii. A map indicating proposed public improvements for future construction on or near the proposed subdivision;
- iv. A map indicating any road, street, park, or public improvement shown on the [municipal comprehensive plan](#).
- v. A written offer of dedication of streets and other public property.
- d. As part of construction plan review, the applicant must obtain approvals from all affected outside agencies. Copies of approval letters and plans must be provided to the [municipality](#) as part of this application.

7.5.6 Final plat

- a. Final plats within subdivisions with common civic space, no more than 60 percent of the lots may be platted prior to the platting of all of the open space specified on the concept plan.
- b. The information required in [Table 7C. Application Requirements](#) is modified as follows:
 - i. Plats must include lot and street numbers.
- c. The following additional information must be provided to supplement the requirements of [Table 7C. Application Requirements](#):
 - i. The final plat of a condominium subdivision must include:
 - (a) The charter and by-laws of any homeowners' association established; and
 - (b) "As built" drawings of all underground utilities for each individual unit.

7.5.7 Building and sign permits

- a. A building permit must be issued before a property owner can begin construction or alteration of any building, structure, or swimming pool.
- b. A site plan must accompany all authorized building permit applications, providing the following:
 - i. The footprint of all existing and proposed structures, driveways, parking areas, utility

Municipality

locations and easements, public rights-of-way, waterways and bodies, and drainage-ways; and

- ii. Applications must demonstrate compliance with [Element 5 Zoning Regulations](#).

TABLE 7C. APPLICATION REQUIREMENTS

	Rezoning	Concept Plan	Preliminary Plat	Construction Plans	Final Plat
General Information					
Applicant and preparer's information	■	■	■	■	Stamped
Property legal description	■				
Date	■	■	■	■	■
Location map		1" = 2,000'	Subdivision	Within 1,000'	
Title, north arrow, and scale	Required for all maps, plans, and drawings				
Maximum drawing scale	1" = 400'	1" = 400'	1" = 100'	1" = 100'	1" = 100'
Existing Site Conditions					
Topography, max. contour	20 ft.	5 ft.	2 ft.	2 ft., within 50' of site	2 ft.
Dimensioned property lines	■	■	■	Inc. bearings	Inc. bearings
Street & alley names and widths	Adjoining	Within 250'	Within 250'	Within 100'	Adjoining
Registered historic places	■	■	■	■	■
Existing structures	■	■	■	■	■
1% floodplain limits	■	■	■	■	■
Wetlands and water bodies		■	■	■	■
Natural features to be preserved or removed		■	■	■	
Bridges, ditches, and all other prominent site features			■	■	■
Adjoining property zoning, owners, and subdivision names	■	■	Within 250'	■	■
Proposal Details					
Community unit designations	■	■	■		
Zoning district designations	■	■	■	■	■
Streets and alleys		Location	Dimensioned	Dimensioned	Dimensioned
New lots		Location	Dimensioned	Dimensioned	Dimensioned
Civic spaces		Location	Dimensioned	Dimensioned	Dimensioned
Utilities and easements	Easements	Easements	■	■	■
Drainage plan and structures			■	■	
Phasing and construction traffic			■	■	
Public landscape and lighting				■	
Grading plan and monuments				■	
Sequencing & erosion control				■	
Subdivision name				■	■
Deed restrictions and covenants				■	■

7.6 Deviations

- 7.6.1 There are two types of deviation from the requirements of this [chapter](#): administrative waivers and variances. Whether a deviation requires an administrative waiver or variance is determined by the [planning director](#) or their designee.
- 7.6.2 The request for an administrative waiver or variance only subjects that portion necessary to rule on the specific issue requiring the relief to a public hearing.
- 7.6.3 Administrative waivers
- a. Administrative waivers may permit a practice that is not consistent with a specific provision of this [chapter](#) but is justified by its' intent according to [Table 7E. Administrative Waiver Criteria](#).
 - b. The [planning director](#) or their designee has the authority to approve or disapprove a request for an administrative waiver.
 - c. Eligibility is limited to deviations from specific written standards in this chapter;
 - d. The [planning director](#) or their designee must find that:
 - i. The waiver is consistent with the intent of this [chapter](#);
 - ii. The waiver is consistent with the [municipal comprehensive plan](#);
 - iii. The waiver will not materially endanger the public health or safety or constitute a public nuisance if developed according to the information submitted;
 - iv. The waiver will not substantially injure the value of adjoining property, or that the use is a public necessity;
 - v. The location and character of the use will be in harmony with proximate land uses, and consistent with the purposes of the district;
 - vi. The waiver will advance pedestrian activity; and
 - vii. The waiver will provide for the enhancement, coordination or demarcation between the public and private realm.
 - e. Decisions regarding an administrative waiver must state in writing the reasons for the decision, and must be delivered to the applicant by either first class mail or electronically.
 - f. Appeals of the decision may be made to the [board of appeals](#).

Municipality

7.6.4 Variances

a. [insert local variance protocol]

TABLE 7E. ADMISTRATIVE WAIVER CRITERIA

Relief Type	Required findings	Allowed relief
Topographic constraints		
Block perimeter	Topographic constraints or adjacent ownership limit the ability to create an interconnected network of streets and blocks.	20% max.
Lot dimensions		
Lot width greater than the maximum permitted width	An existing parcel can be developed following the intent of the district and meet all other applicable standards of the district.	20% max.
A decrease in the minimum required parcel width.	An existing parcel can be developed following the intent of the zone and meet all other applicable standards of the zone.	10% max.
Parking location		
Parking location setback from building facades	The lot is wider than 35 feet; The lot is sloped more than 10% uphill away from the sidewalk; The ground floor of the main building is no more than 6 feet above sidewalk grade; Parking is proposed to be entirely enclosed under the main building; The proposed garage door is 9 feet wide or less.	Reduction in the parking location setback from building facades to equal the main building front setback.
Setbacks		
A decrease of the minimum setback or increase to the maximum setback.	Existing development on adjacent parcels on the same block face is more similar to the proposed setback than the required setback; The modulation will allow the proposed building to blend in with the adjacent buildings.	5' or 40% max., whichever is greater.
A relaxation of the specified build-to-line.	Existing development on adjacent parcels on the same block face is more similar to the proposed setback than the required setback; The modulation will allow the proposed building to blend in with the adjacent buildings.	2' max.
Allow buildings to be placed closer or further from a parcel line due to existing site features, such as trees, watercourses or topographical changes.	Existing site features would be negatively impacted if buildings follow the required setback, or The constraint of existing site features would not allow for construction of habitable spaces within buildings.	20% max.
Building placement		
A relaxation in the amount of the lot width building facades must occupy.	The proposal does not expose additional parking towards sidewalks; The lot is greater than 100 feet wide.	10% max.

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Municipality

ELEMENT 8 DEFINITIONS

8.1 Definitions

This Section provides definitions for terms in this [chapter](#) that are technical in nature or that may not reflect a common usage of the term. Items in italics refer to other terms defined in this [Element](#).

A-grid: cumulatively, those streets that by virtue of their pre-existing pedestrian-supportive qualities, or their future importance to pedestrian connectivity, are held to the highest standards prescribed by this Code. See *B-grid*.

Accessory Building: a structure which is on the same parcel of property as a principal structure and the use of which is incidental to the use of the principal structure. For example a residential structure may have a detached garage, storage shed, or guest house.

Accessory Dwelling Unit (ADU): a rental dwelling unit not greater than 600 square feet, sharing ownership and utility connections with a principal building; it may be within an outbuilding or within the principal building. (Syn: ancillary unit)

Allee: a regularly spaced and aligned row of trees usually planted along a street or path.

Alley: an access easement, designated to be a secondary means of vehicular access to the rear or side of properties; an *Alley* may connect to a vehicular driveway located to the rear of lots providing access to outbuildings, service areas and parking, and containing utility easements.

Arcade: a frontage encroachment wherein the facade is a colonnade supporting habitable space that overlaps the sidewalk, while the facade at sidewalk level remains at the frontage line.

Attic: the interior part of a building contained within a pitched roof structure.

Avenue (AV): A limited distance, free-movement street connecting civic locations within an urbanized area. Unlike a boulevard, its length is finite and its axis is terminated. An avenue may be conceived as an elongated square.

Awning: a fixed or movable shading structure, cantilevered or otherwise entirely supported from a building, used to protect outdoor spaces from sun, rain, and other natural conditions. Awnings are typically used to cover outdoor seating for restaurants and cafes.

B-Grid: cumulatively, those streets that by virtue of their use, location, or absence of pre-existing pedestrian-supportive qualities, may meet a standard lower than that of the *A-grid*. See *A-Grid*.

Bicycle Lane (BL): a dedicated lane for cycling within a street, demarcated by striping.

Bicycle Route (BR): a street suitable for the shared use of bicycles and automobiles moving at low speeds.

Bicycle Trail (BT): a bicycle way independent of a vehicular way.

Blade Sign: a sign made from rigid material mounted perpen-

dicular to a building wall with one side attached or supported by a device extending from a building wall.

Block: the aggregate of private lots, civic zones, passages, alleys and rear lanes, circumscribed by streets.

Block Face: the aggregate of all the principal frontages on one side of a block.

Boulevard (BV): A long-distance street traversing an urbanized area. A boulevard often includes a wide median and a wide public frontage. Access lanes are often provided to separate public frontages from the higher speed lanes.

Building Height: the vertical extent of a building measured in stories.

Bus Rapid Transit (BRT): a rubber tire vehicular transportation system with its own right-of-way or dedicated lane along its route, providing service that is faster than a regular bus.

Civic: the term defining not-for-profit organizations dedicated to arts, culture, education, recreation, government, municipal government, and transit.

Civic Building: a building operated by one or more civic organizations.

Civic Space: an outdoor area permanently dedicated for public use, operated by a civic organization.

Commercial: the term collectively defining workplace, office, retail, and lodging functions.

Commercial Street (CS): a local, slow-movement street suitable for high-intensity urban areas. Commercial streets are urban in character, supporting parallel or angled parking on both sides and narrow lanes appropriate for a commercial environment.

Common Destination: an area of focused community activity, usually defining the approximate center of a pedestrian shed. It may include without limitation one or more of the following: a civic space, a civic building, a commercial center, a main street, or a transit station, and may act as the social center of a neighborhood.

Common Yard: a planted private frontage, visually continuous with adjacent yards, wherein the facade is set back from the frontage line.

Community Unit: a regulatory boundary mapped on a structure plan.

Consolidated review committee (CRC): Part of the Community Development Department, a CRC is comprised of a representative from each of the various regulatory agencies that have jurisdiction over the permitting of a project. See [§7.1.5](#).

Corner Lot: a lot or parcel of land abutting two (2) or more streets at their intersection, or two (2) parts of the same street forming an interior angle of less than one hundred thirty-five (135) degrees.

CRC: Consolidated Review Committee. See [§7.1.5](#).

Curb: the edge of the vehicular pavement that may be raised,

Municipality

usually incorporates the drainage system.

Curb Return Radius: the curved edge of a street at an intersection, measured at the inside edge of the vehicular tracking along the curb.

Density: the number of housing units within a standard measure of land area.

Design Speed: the velocity at which a street tends to be driven without the constraints of signage or enforcement.

Drive (DR): a street along the boundary between an urbanized and a natural condition or civic zone, usually along a waterfront, park, or promontory. One side has the urban character of a street or boulevard, while the other has the qualities of a road, with naturalistic planting and rural details.

Driveway: a vehicular lane within a lot, often leading to a garage.

Elevation: an exterior wall of a building not along a frontage line. See facade.

Encroach: to break the plane of a vertical or horizontal regulatory limit with a structural element, so that it extends into a setback, into the public frontage, or above a height limit.

Encroachment: any structural element that encroaches.

Exception: a ruling that would permit a practice that is not consistent with a specific provision of this Section but is justified by its Intent.

Facade: the exterior wall of a building that is set along a frontage line.

Fence: a permeable metal or wooden wall, independent of a building.

Forecourt: a private frontage wherein a portion of the facade is close to the frontage line and the central portion is set back.

Front Setback: the distance from the frontage line to the point where a building may be constructed. This area must be maintained clear of permanent structures with the exception of encroachments.

Frontage: the area between a building facade and the vehicular lanes, inclusive of its built and planted components. Frontage is divided into private frontage and public frontage.

Frontage, Primary: corner lots have two frontages. The primary frontage private frontage and public frontage. The primary frontage faces the street of the property address.

Frontage, Secondary: the frontage facing the non-addressed street.

Frontage Line: a lot line bordering a public frontage.

Gallery: a private frontage wherein the facade is aligned close to the frontage line with an attached cantilevered shed or light-weight colonnade overlapping the sidewalk.

Green: a civic space type for unstructured recreation, spatially defined by landscaping rather than building frontages.

Hamlet (HAM): a community unit type structured by a short or standard pedestrian shed oriented toward a common destination such as a general store, meeting hall, schoolhouse, and / or church. A hamlet takes the form of a small settlement standing free in the countryside. (Syn: conservation land development, cluster)

Height: See building height.

Highway (HW): a rural and suburban road of long distance and high vehicular speed and capacity. A highway should be relatively free of intersections, driveways, and adjacent buildings. This type is allocated to the more rural transect zones (T-1, T-2, and T-3).

Home Occupation: non-retail commercial enterprises permitted under the Restricted office category.

House: an edgeyard building type, usually a single-family dwelling on a large lot, often shared with an *accessory building* in the back yard. (Syn: single.)

Industrial: the function associated with a business or activity involving manufacturing, fabrication, assembly, distribution, disposal, warehousing or bulk storage, trucking and equipment facilities, and other business serving primarily industrial needs.

Lightwell: A private frontage type that is a below-grade entrance or recess designed to allow light into basements. (Syn: light court.)

Liner Building: a building specifically designed to mask a parking lot or a parking structure from a frontage.

Live-Work: a mixed-use unit consisting of a commercial and residential function. The commercial function may be anywhere in the unit. It is intended to be occupied by a business operator who lives in the same structure that contains the commercial activity or industry.

Lodging: a building function available for daily and weekly renting of bedrooms.

Lot: a parcel of land accommodating a building or buildings under single ownership.

Lot Coverage: the percentage of a lot that is covered by buildings and other roofed structures.

Lot Line: the boundary that legally and geometrically demarcates a lot.

Lot Occupation: category for the area of a lot that may contain buildings, regulated in terms of width and coverage metrics.

Lot Width: the length of the principal frontage line of a lot.

Main Civic Space: the primary outdoor gathering place for a community. The main civic space is often, but not always, associated with an important civic building.

Manufacturing: premises available for the creation, assemblage, and repair of artifacts, using table-mounted electrical machinery or artisanal equipment, and including their retail sale.

Municipality

Meeting Hall: a building available for gatherings, including conferences, that accommodates at least one room equivalent to a minimum of 10 square feet per projected dwelling unit within the pedestrian shed in which it is located.

Mixed Use: multiple functions within the same building through superimposition or adjacency, or in multiple buildings by adjacency, or at a proximity determined by warrant.

Neighborhood Multipurpose Field: a civic space type for structured recreation and stormwater management. It may be spatially defined by landscaping rather than building frontages.

Net Lot Area: the area of a lot within the frontage lines, excluding any portions of street rights-of-way or other required dedications.

Office: premises available for the transaction of general business but excluding retail, artisanal, and manufacturing uses.

Open Space: land intended to remain undeveloped; it may be for civic space.

Outbuilding: an *accessory building*, usually located toward the rear of the same lot as a principal building, and sometimes connected to the principal building by a backbuilding.

Overlay District: a zone mapped onto one or more zones that modifies metrics and regulations of the underlying zones.

Park: a civic space type that is a natural preserve available for unstructured recreation.

Parking Lane: A vehicular lane designated and used for parking motor vehicles.

Parking Structure: a building containing one or more stories of vehicular parking above or below grade.

Passage (PS): a pedestrian connector, open or roofed, that passes between buildings to provide shortcuts through long blocks and connect rear parking areas to frontages.

Path (PT): a pedestrian way traversing a park or rural area, with landscape matching the contiguous open space, ideally connecting directly with the urban sidewalk network.

Pedestrian Crossing: an area designated for pedestrians when traversing a street. Pedestrian crossings should be striped and indicated by signage.

Pedestrian Shed: An area that is centered on a common destination. It is an average 1/4 mile radius or 1320 feet, about the distance of a five-minute walk at a leisurely pace. Pedestrian sheds are applied to structure Communities. (Syn: walkshed, walkable catchment.)

Planter: the element of the public frontage which accommodates street trees, whether continuous or individual.

Playground: an open space designed and equipped for the recreation of children.

Plaza: a civic space type designed for civic purposes and commercial activities in the more urban transect zones, generally paved and spatially defined by building frontages.

Porch: An open air room appended to a building, with floor and roof but no walls on the sides facing frontages.

Principal Building: the main building on a lot, usually located toward the frontage.

Principal Entrance: the main point of access for pedestrians into a building.

Principal Frontage: the private frontage designated to bear the address and principal entrance to the building, and the measure of minimum lot width. See frontage.

Private Frontage: the privately held area between the frontage line and the principal building facade.

Public Frontage: the area between the curb of the vehicular lanes and the frontage line.

Rear Alley (RA): a vehicular way located to the rear of lots providing access to service areas, parking, and Outbuildings and containing utility easements. rear alleys should be paved from building face to building face, with drainage by inverted crown at the center or with roll Curbs at the edges. (Disambiguation See *Alley*.)

Rear Lane (RL): a vehicular way located to the rear of lots providing access to service areas, parking, and outbuildings and containing utility easements. Rear lanes may be paved lightly to driveway standards. The streetscape consists of gravel or landscaped edges, no raised curb, and is drained by percolation.

Rear Setback: the distance from the rear lot line to the point where a building may be constructed. This area must be maintained clear of permanent structures with the exception of encroachments.

Regulating Plan: a zoning map or set of maps that show the transect zones, civic zones, special districts, special requirements, and street assignments of areas subject to, or potentially subject to, regulation by this Section.

Residential: characterizing premises available for long-term human dwelling.

Retail: characterizing premises available for the sale of merchandise and food service.

Retail Frontage: a frontage designated on a regulating plan that requires or recommends the provision of a shopfront, encouraging the ground level to be available for retail use. See special requirements.

Road (RD): a local, rural, and suburban street of low-to-moderate vehicular speed and capacity. Roads tend to be rural in character without curbs or striping. (ALT) a local street of low-to-moderate vehicular speed and capacity and of rural character, without curbs or striping.

Secondary Frontage: on corner lots, the private frontage that is not the principal frontage. As it affects the public realm, its first layer is regulated.

Municipality

Setback: the area of a lot measured from the lot line to a building facade or elevation that is maintained clear of permanent structures, with the exception of encroachments. (Var: build-to-line.)

Shopfront: a private frontage conventional for retail use with substantial glazing wherein the facade is aligned close to the frontage line with the building entrance at sidewalk grade.

Sidewalk: the paved section of the public frontage dedicated exclusively to pedestrian activity. (Syn. walkway)

Sign Band: a wall area of a non-residential building built along the entire width of a principal or secondary frontage allocated for the placement of a sign above a shopfront or at the cornice.

Slip Lane: an outer vehicular lane or lanes of a street, designed for slow speeds and separated from inner lanes that carry higher speed traffic. (Syn: access lane, slip road.)

Special District (SD): an area that, by its intrinsic function, disposition, or configuration, cannot or should not conform to one or more of the normative community unit types or transect zones.

Special Requirements: provisions of this Section that modify or extend requirements as designated on a regulating plan or other map for those provisions.

Square: a civic space type designed for unstructured recreation and civic purposes, spatially defined by building frontages and consisting of paths, lawns and trees, formally disposed.

Stoop: a private frontage wherein the facade is aligned close to the frontage line with the first story elevated from the sidewalk for privacy, with an exterior stair and landing at the entrance.

Story: a habitable level within a building by which height is measured, excluding an attic or raised basement.

Street (ST): a way for use by vehicular and pedestrian traffic and to provide access to lots and open spaces, consisting of vehicular lanes and the public frontage.

Street Assembly: the result of the public frontage elements assembled with the elements within a right-of-way.

Street Network: an interconnected network of vehicular, pedestrian and bicycle mobility.

Streetscreen: a freestanding wall built along the frontage line, or coplanar with a facade. (Syn: streetwall)

Substantial Modification: alteration to a building that is valued at more than 50% of the replacement cost of the entire building, if new.

Swale: a low or slightly depressed natural area for drainage.

Target Speed: the desirable velocity at which vehicles should travel on a street, according to each T-Zone. (adapted from the ITE)

Terrace: A frontage wherein the facade is set back from the frontage line by an elevated terrace or a sunken lightwell. This type buffers residential use from urban sidewalks and removes the private yard from public encroachment. (Syn: lighwell)

Terminated Vista: a location at the axial conclusion of a street.

Transect: a cross-section of the environment showing a range of different habitats. The rural-urban transect of the human environment used in this Section is divided into six transect zones. These zones describe the physical form and character of a place, according to the density and intensity of its land use.

Transect District: One of several areas on a zoning map regulated by this code. Transect districts are administratively similar to the land use zones in conventional codes, except that in addition to the usual building use, density, height, and setback requirements, other elements of the intended habitat are integrated, including those of the private lot, building, and public frontage.

Transit Overlay District: TOD is created by an overlay on all or part of one or more community units, modifying regulations of this Section to support rail or bus rapid transit (BRT).

Turning Radius: the curved edge of a street at an intersection, measured at the inside edge of the vehicular tracking. The smaller the turning radius, the smaller the pedestrian crossing distance and the more slowly the vehicle is forced to make the turn.

Urbanized: generally, developed. Specific to the SmartCode, developed at T-3 (Sub-Urban) density or higher.

Vehicular Lane: the portion of a thoroughfare which is occupied by vehicles, usually the moving lanes and parking lanes. The vehicular lane together with the walkway fill the right-of-way.

Village (VIL): a community unit type structured by a standard pedestrian shed oriented toward a common destination consisting of a mixed use center or corridor, and in the form of a medium-sized settlement near a transportation route. (Syn: traditional neighborhood development. neighborhood.)

Walkway: the portion of the street dedicated exclusively to pedestrian activity. The walkway includes sidewalks and the planting areas of the streetscape. (Syn. Sidewalk)

Yield: characterizing a street that has two-way traffic but only one effective travel lane because of parked cars, necessitating slow movement and driver negotiation. Also, characterizing parking on such a thoroughfare.

Zoning Map: the official map or maps that are part of the zoning ordinance and delineate the boundaries of individual zones and districts. See regulating plan.